

Full Environmental Assessment Form Part 1

TOWN OF MALTA

NEIGHBORHOOD 7 STUDY AREA

**Comprehensive Master Plan Amendment,
Rezoning, and New Route 67 West Overlay**

Town of Malta
Saratoga County, New York

September 9, 2016



Engineers
Land Surveyors
Planners
Environmental Professionals
Landscape Architects

Prepared for:
Town of Malta
2540 Route 9
Malta, NY 12020

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PROJECT DESCRIPTION

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The Town of Malta is proposing an amendment to Neighborhood 7 of the Town of Malta Comprehensive Master Plan adopted in 2005 and a rezoning of parcels along NYS Route 67 consistent with the proposed Comprehensive Plan amendment.

The Town Board appointed a Route 67 West Comprehensive Master Plan Committee on March 21, 2016, to prepare recommendations for zoning and policy changes for the Neighborhood 7 study area (approximately 630 acres). Committee meetings were conducted at Town Hall with members of the public present and participating. Collectively the committee reviewed current zoning, planning documents, policies, plans, reports and surrounding land uses and zoning. Public input was solicited, draft provisions were crafted, and revisions to address comment and committee input were completed. The Committee has presented a copy of the Comprehensive Plan amendment and rezoning proposals to the Town Board for their consideration.

The proposed amendment to the Comprehensive Plan and zoning regulations reflect the Town's desire to plan for and accommodate growth along the Route 67 corridor, while also addressing changing land use characteristic, increases in traffic, the land use plans of adjoining communities, and the Town's vision for the corridor. A copy of the Comprehensive Plan Amendment is provided as Attachment A.

In order to effect the vision of the Town's Plan, the Town propose to rezoning approximately 210 acres of land to Office Commercial (C-2) and establish the Route 67 West Overlay District to be applied to these rezoned parcels. The Route 67 West Overlay District is crafted specifically for the target area and allows for additional uses and eliminates others which are not consistent with this vision. Attachment B provides the proposed amendments to the zoning regulations, and Attachment C contains the proposed Use and Bulk Regulations for C-2 and Route 67 West Overlay. Attachment D is the draft Route 67 West Overlay District Design Standards & Guidelines. Figures depicting the area of rezoning are provided, and a list of the affected parcels is provided as Attachment E.

FULL ENVIRONMENTAL ASSESSMENT FORM (FEAF)

Full Environmental Assessment Form
Part 1 - Project and Setting

Instructions for Completing Part 1

Part 1 is to be completed by the applicant or project sponsor. Responses become part of the application for approval or funding, are subject to public review, and may be subject to further verification.

Complete Part 1 based on information currently available. If additional research or investigation would be needed to fully respond to any item, please answer as thoroughly as possible based on current information; indicate whether missing information does not exist, or is not reasonably available to the sponsor; and, when possible, generally describe work or studies which would be necessary to update or fully develop that information.

Applicants/sponsors must complete all items in Sections A & B. In Sections C, D & E, most items contain an initial question that must be answered either “Yes” or “No”. If the answer to the initial question is “Yes”, complete the sub-questions that follow. If the answer to the initial question is “No”, proceed to the next question. Section F allows the project sponsor to identify and attach any additional information. Section G requires the name and signature of the project sponsor to verify that the information contained in Part 1 is accurate and complete.

A. Project and Sponsor Information.

Name of Action or Project:		
Project Location (describe, and attach a general location map):		
Brief Description of Proposed Action (include purpose or need):		
Name of Applicant/Sponsor:		Telephone:
		E-Mail:
Address:		
City/PO:	State:	Zip Code:
Project Contact (if not same as sponsor; give name and title/role):		Telephone:
		E-Mail:
Address:		
City/PO:	State:	Zip Code:
Property Owner (if not same as sponsor):		Telephone:
		E-Mail:
Address:		
City/PO:	State:	Zip Code:

B. Government Approvals

B. Government Approvals, Funding, or Sponsorship. (“Funding” includes grants, loans, tax relief, and any other forms of financial assistance.)

Government Entity	If Yes: Identify Agency and Approval(s) Required	Application Date (Actual or projected)
a. City Council, Town Board, or Village Board of Trustees <input type="checkbox"/> Yes <input type="checkbox"/> No		
b. City, Town or Village Planning Board or Commission <input type="checkbox"/> Yes <input type="checkbox"/> No		
c. City Council, Town or Village Zoning Board of Appeals <input type="checkbox"/> Yes <input type="checkbox"/> No		
d. Other local agencies <input type="checkbox"/> Yes <input type="checkbox"/> No		
e. County agencies <input type="checkbox"/> Yes <input type="checkbox"/> No		
f. Regional agencies <input type="checkbox"/> Yes <input type="checkbox"/> No		
g. State agencies <input type="checkbox"/> Yes <input type="checkbox"/> No		
h. Federal agencies <input type="checkbox"/> Yes <input type="checkbox"/> No		
i. Coastal Resources. <ul style="list-style-type: none"> <li data-bbox="121 829 1485 861">i. Is the project site within a Coastal Area, or the waterfront area of a Designated Inland Waterway? <input type="checkbox"/> Yes <input type="checkbox"/> No <li data-bbox="121 892 1485 924">ii. Is the project site located in a community with an approved Local Waterfront Revitalization Program? <input type="checkbox"/> Yes <input type="checkbox"/> No <li data-bbox="121 924 1485 955">iii. Is the project site within a Coastal Erosion Hazard Area? <input type="checkbox"/> Yes <input type="checkbox"/> No 		

C. Planning and Zoning

C.1. Planning and zoning actions.

Will administrative or legislative adoption, or amendment of a plan, local law, ordinance, rule or regulation be the only approval(s) which must be granted to enable the proposed action to proceed? Yes No

- **If Yes**, complete sections C, F and G.
- **If No**, proceed to question C.2 and complete all remaining sections and questions in Part 1

C.2. Adopted land use plans.

a. Do any municipally- adopted (city, town, village or county) comprehensive land use plan(s) include the site where the proposed action would be located? Yes No

If Yes, does the comprehensive plan include specific recommendations for the site where the proposed action would be located? Yes No

b. Is the site of the proposed action within any local or regional special planning district (for example: Greenway Brownfield Opportunity Area (BOA); designated State or Federal heritage area; watershed management plan; or other?) Yes No

If Yes, identify the plan(s):

c. Is the proposed action located wholly or partially within an area listed in an adopted municipal open space plan, or an adopted municipal farmland protection plan? Yes No

If Yes, identify the plan(s):

C.3. Zoning

a. Is the site of the proposed action located in a municipality with an adopted zoning law or ordinance. Yes No
If Yes, what is the zoning classification(s) including any applicable overlay district?

b. Is the use permitted or allowed by a special or conditional use permit? Yes No

c. Is a zoning change requested as part of the proposed action? Yes No

If Yes,

i. What is the proposed new zoning for the site? _____

C.4. Existing community services.

a. In what school district is the project site located? _____

b. What police or other public protection forces serve the project site?

c. Which fire protection and emergency medical services serve the project site?

d. What parks serve the project site?

D. Project Details Not Applicable per C.1.

D.1. Proposed and Potential Development

a. What is the general nature of the proposed action (e.g., residential, industrial, commercial, recreational; if mixed, include all components)?

b. a. Total acreage of the site of the proposed action? _____ acres
b. Total acreage to be physically disturbed? _____ acres
c. Total acreage (project site and any contiguous properties) owned or controlled by the applicant or project sponsor? _____ acres

c. Is the proposed action an expansion of an existing project or use? Yes No
i. If Yes, what is the approximate percentage of the proposed expansion and identify the units (e.g., acres, miles, housing units, square feet)? % _____ Units: _____

d. Is the proposed action a subdivision, or does it include a subdivision? Yes No

If Yes,

i. Purpose or type of subdivision? (e.g., residential, industrial, commercial; if mixed, specify types)

ii. Is a cluster/conservation layout proposed? Yes No

iii. Number of lots proposed? _____

iv. Minimum and maximum proposed lot sizes? Minimum _____ Maximum _____

e. Will proposed action be constructed in multiple phases? Yes No

i. If No, anticipated period of construction: _____ months

ii. If Yes:

- Total number of phases anticipated _____
- Anticipated commencement date of phase 1 (including demolition) _____ month _____ year
- Anticipated completion date of final phase _____ month _____ year

• Generally describe connections or relationships among phases, including any contingencies where progress of one phase may determine timing or duration of future phases: _____

f. Does the project include new residential uses? Yes No
 If Yes, show numbers of units proposed.

	<u>One Family</u>	<u>Two Family</u>	<u>Three Family</u>	<u>Multiple Family (four or more)</u>
Initial Phase	_____	_____	_____	_____
At completion	_____	_____	_____	_____
of all phases	_____	_____	_____	_____

g. Does the proposed action include new non-residential construction (including expansions)? Yes No
 If Yes,

i. Total number of structures _____

ii. Dimensions (in feet) of largest proposed structure: _____ height; _____ width; and _____ length

iii. Approximate extent of building space to be heated or cooled: _____ square feet

h. Does the proposed action include construction or other activities that will result in the impoundment of any liquids, such as creation of a water supply, reservoir, pond, lake, waste lagoon or other storage? Yes No
 If Yes,

i. Purpose of the impoundment: _____

ii. If a water impoundment, the principal source of the water: Ground water Surface water streams Other specify: _____

iii. If other than water, identify the type of impounded/contained liquids and their source.

iv. Approximate size of the proposed impoundment. Volume: _____ million gallons; surface area: _____ acres

v. Dimensions of the proposed dam or impounding structure: _____ height; _____ length

vi. Construction method/materials for the proposed dam or impounding structure (e.g., earth fill, rock, wood, concrete):

D.2. Project Operations

a. Does the proposed action include any excavation, mining, or dredging, during construction, operations, or both? Yes No
 (Not including general site preparation, grading or installation of utilities or foundations where all excavated materials will remain onsite)
 If Yes:

i. What is the purpose of the excavation or dredging? _____

ii. How much material (including rock, earth, sediments, etc.) is proposed to be removed from the site?

- Volume (specify tons or cubic yards): _____
- Over what duration of time? _____

iii. Describe nature and characteristics of materials to be excavated or dredged, and plans to use, manage or dispose of them.

iv. Will there be onsite dewatering or processing of excavated materials? Yes No
 If yes, describe. _____

v. What is the total area to be dredged or excavated? _____ acres

vi. What is the maximum area to be worked at any one time? _____ acres

vii. What would be the maximum depth of excavation or dredging? _____ feet

viii. Will the excavation require blasting? Yes No

ix. Summarize site reclamation goals and plan: _____

b. Would the proposed action cause or result in alteration of, increase or decrease in size of, or encroachment into any existing wetland, waterbody, shoreline, beach or adjacent area? Yes No
 If Yes:

i. Identify the wetland or waterbody which would be affected (by name, water index number, wetland map number or geographic description): _____

ii. Describe how the proposed action would affect that waterbody or wetland, e.g. excavation, fill, placement of structures, or alteration of channels, banks and shorelines. Indicate extent of activities, alterations and additions in square feet or acres:

iii. Will proposed action cause or result in disturbance to bottom sediments? Yes No

If Yes, describe: _____

iv. Will proposed action cause or result in the destruction or removal of aquatic vegetation? Yes No

If Yes:

- acres of aquatic vegetation proposed to be removed: _____
- expected acreage of aquatic vegetation remaining after project completion: _____
- purpose of proposed removal (e.g. beach clearing, invasive species control, boat access): _____
- proposed method of plant removal: _____
- if chemical/herbicide treatment will be used, specify product(s): _____

v. Describe any proposed reclamation/mitigation following disturbance: _____

c. Will the proposed action use, or create a new demand for water? Yes No

If Yes:

i. Total anticipated water usage/demand per day: _____ gallons/day

ii. Will the proposed action obtain water from an existing public water supply? Yes No

If Yes:

- Name of district or service area: _____
- Does the existing public water supply have capacity to serve the proposal? Yes No
- Is the project site in the existing district? Yes No
- Is expansion of the district needed? Yes No
- Do existing lines serve the project site? Yes No

iii. Will line extension within an existing district be necessary to supply the project? Yes No

If Yes:

- Describe extensions or capacity expansions proposed to serve this project: _____
- Source(s) of supply for the district: _____

iv. Is a new water supply district or service area proposed to be formed to serve the project site? Yes No

If Yes:

- Applicant/sponsor for new district: _____
- Date application submitted or anticipated: _____
- Proposed source(s) of supply for new district: _____

v. If a public water supply will not be used, describe plans to provide water supply for the project: _____

vi. If water supply will be from wells (public or private), maximum pumping capacity: _____ gallons/minute.

d. Will the proposed action generate liquid wastes? Yes No

If Yes:

i. Total anticipated liquid waste generation per day: _____ gallons/day

ii. Nature of liquid wastes to be generated (e.g., sanitary wastewater, industrial; if combination, describe all components and approximate volumes or proportions of each): _____

iii. Will the proposed action use any existing public wastewater treatment facilities? Yes No

If Yes:

- Name of wastewater treatment plant to be used: _____
- Name of district: _____
- Does the existing wastewater treatment plant have capacity to serve the project? Yes No
- Is the project site in the existing district? Yes No
- Is expansion of the district needed? Yes No

- Do existing sewer lines serve the project site? Yes No
- Will line extension within an existing district be necessary to serve the project? Yes No

 If Yes:

- Describe extensions or capacity expansions proposed to serve this project: _____

iv. Will a new wastewater (sewage) treatment district be formed to serve the project site? Yes No
 If Yes:

- Applicant/sponsor for new district: _____
- Date application submitted or anticipated: _____
- What is the receiving water for the wastewater discharge? _____

v. If public facilities will not be used, describe plans to provide wastewater treatment for the project, including specifying proposed receiving water (name and classification if surface discharge, or describe subsurface disposal plans):

vi. Describe any plans or designs to capture, recycle or reuse liquid waste: _____

e. Will the proposed action disturb more than one acre and create stormwater runoff, either from new point sources (i.e. ditches, pipes, swales, curbs, gutters or other concentrated flows of stormwater) or non-point source (i.e. sheet flow) during construction or post construction? Yes No
 If Yes:

- How much impervious surface will the project create in relation to total size of project parcel?
 _____ Square feet or _____ acres (impervious surface)
 _____ Square feet or _____ acres (parcel size)
- Describe types of new point sources. _____

- Where will the stormwater runoff be directed (i.e. on-site stormwater management facility/structures, adjacent properties, groundwater, on-site surface water or off-site surface waters)?

 - If to surface waters, identify receiving water bodies or wetlands: _____

 - Will stormwater runoff flow to adjacent properties? Yes No

iv. Does proposed plan minimize impervious surfaces, use pervious materials or collect and re-use stormwater? Yes No

f. Does the proposed action include, or will it use on-site, one or more sources of air emissions, including fuel combustion, waste incineration, or other processes or operations? Yes No
 If Yes, identify:

- Mobile sources during project operations (e.g., heavy equipment, fleet or delivery vehicles)

- Stationary sources during construction (e.g., power generation, structural heating, batch plant, crushers)

- Stationary sources during operations (e.g., process emissions, large boilers, electric generation)

g. Will any air emission sources named in D.2.f (above), require a NY State Air Registration, Air Facility Permit, or Federal Clean Air Act Title IV or Title V Permit? Yes No
 If Yes:

- Is the project site located in an Air quality non-attainment area? (Area routinely or periodically fails to meet ambient air quality standards for all or some parts of the year) Yes No
- In addition to emissions as calculated in the application, the project will generate:
 - _____ Tons/year (short tons) of Carbon Dioxide (CO₂)
 - _____ Tons/year (short tons) of Nitrous Oxide (N₂O)
 - _____ Tons/year (short tons) of Perfluorocarbons (PFCs)
 - _____ Tons/year (short tons) of Sulfur Hexafluoride (SF₆)
 - _____ Tons/year (short tons) of Carbon Dioxide equivalent of Hydroflouorocarbons (HFCs)
 - _____ Tons/year (short tons) of Hazardous Air Pollutants (HAPs)

h. Will the proposed action generate or emit methane (including, but not limited to, sewage treatment plants, landfills, composting facilities)? Yes No

If Yes:

i. Estimate methane generation in tons/year (metric): _____

ii. Describe any methane capture, control or elimination measures included in project design (e.g., combustion to generate heat or electricity, flaring): _____

i. Will the proposed action result in the release of air pollutants from open-air operations or processes, such as quarry or landfill operations? Yes No

If Yes: Describe operations and nature of emissions (e.g., diesel exhaust, rock particulates/dust): _____

j. Will the proposed action result in a substantial increase in traffic above present levels or generate substantial new demand for transportation facilities or services? Yes No

If Yes:

i. When is the peak traffic expected (Check all that apply): Morning Evening Weekend
 Randomly between hours of _____ to _____.

ii. For commercial activities only, projected number of semi-trailer truck trips/day: _____

iii. Parking spaces: Existing _____ Proposed _____ Net increase/decrease _____

iv. Does the proposed action include any shared use parking? Yes No

v. If the proposed action includes any modification of existing roads, creation of new roads or change in existing access, describe: _____

vi. Are public/private transportation service(s) or facilities available within 1/2 mile of the proposed site? Yes No

vii. Will the proposed action include access to public transportation or accommodations for use of hybrid, electric or other alternative fueled vehicles? Yes No

viii. Will the proposed action include plans for pedestrian or bicycle accommodations for connections to existing pedestrian or bicycle routes? Yes No

k. Will the proposed action (for commercial or industrial projects only) generate new or additional demand for energy? Yes No

If Yes:

i. Estimate annual electricity demand during operation of the proposed action: _____

ii. Anticipated sources/suppliers of electricity for the project (e.g., on-site combustion, on-site renewable, via grid/local utility, or other): _____

iii. Will the proposed action require a new, or an upgrade to, an existing substation? Yes No

l. Hours of operation. Answer all items which apply.

<p><i>i.</i> During Construction:</p> <ul style="list-style-type: none"> • Monday - Friday: _____ • Saturday: _____ • Sunday: _____ • Holidays: _____ 	<p><i>ii.</i> During Operations:</p> <ul style="list-style-type: none"> • Monday - Friday: _____ • Saturday: _____ • Sunday: _____ • Holidays: _____
---	--

m. Will the proposed action produce noise that will exceed existing ambient noise levels during construction, operation, or both? Yes No
 If yes:
 i. Provide details including sources, time of day and duration:

ii. Will proposed action remove existing natural barriers that could act as a noise barrier or screen? Yes No
 Describe: _____

n.. Will the proposed action have outdoor lighting? Yes No
 If yes:
 i. Describe source(s), location(s), height of fixture(s), direction/aim, and proximity to nearest occupied structures:

ii. Will proposed action remove existing natural barriers that could act as a light barrier or screen? Yes No
 Describe: _____

o. Does the proposed action have the potential to produce odors for more than one hour per day? Yes No
 If Yes, describe possible sources, potential frequency and duration of odor emissions, and proximity to nearest occupied structures: _____

p. Will the proposed action include any bulk storage of petroleum (combined capacity of over 1,100 gallons) or chemical products 185 gallons in above ground storage or any amount in underground storage? Yes No
 If Yes:
 i. Product(s) to be stored _____
 ii. Volume(s) _____ per unit time _____ (e.g., month, year)
 iii. Generally describe proposed storage facilities: _____

q. Will the proposed action (commercial, industrial and recreational projects only) use pesticides (i.e., herbicides, insecticides) during construction or operation? Yes No
 If Yes:
 i. Describe proposed treatment(s):

ii. Will the proposed action use Integrated Pest Management Practices? Yes No

r. Will the proposed action (commercial or industrial projects only) involve or require the management or disposal of solid waste (excluding hazardous materials)? Yes No
 If Yes:
 i. Describe any solid waste(s) to be generated during construction or operation of the facility:
 • Construction: _____ tons per _____ (unit of time)
 • Operation : _____ tons per _____ (unit of time)
 ii. Describe any proposals for on-site minimization, recycling or reuse of materials to avoid disposal as solid waste:
 • Construction: _____

 • Operation: _____

iii. Proposed disposal methods/facilities for solid waste generated on-site:
 • Construction: _____

 • Operation: _____

s. Does the proposed action include construction or modification of a solid waste management facility? Yes No
 If Yes:
 i. Type of management or handling of waste proposed for the site (e.g., recycling or transfer station, composting, landfill, or other disposal activities): _____
 ii. Anticipated rate of disposal/processing:
 • _____ Tons/month, if transfer or other non-combustion/thermal treatment, or
 • _____ Tons/hour, if combustion or thermal treatment
 iii. If landfill, anticipated site life: _____ years

t. Will proposed action at the site involve the commercial generation, treatment, storage, or disposal of hazardous waste? Yes No
 If Yes:
 i. Name(s) of all hazardous wastes or constituents to be generated, handled or managed at facility: _____

 ii. Generally describe processes or activities involving hazardous wastes or constituents: _____

 iii. Specify amount to be handled or generated _____ tons/month
 iv. Describe any proposals for on-site minimization, recycling or reuse of hazardous constituents: _____

 v. Will any hazardous wastes be disposed at an existing offsite hazardous waste facility? Yes No
 If Yes: provide name and location of facility: _____

 If No: describe proposed management of any hazardous wastes which will not be sent to a hazardous waste facility:

E. Site and Setting of Proposed Action [Not Applicable per C.1.](#)

E.1. Land uses on and surrounding the project site

a. Existing land uses.
 i. Check all uses that occur on, adjoining and near the project site.
 Urban Industrial Commercial Residential (suburban) Rural (non-farm)
 Forest Agriculture Aquatic Other (specify): _____
 ii. If mix of uses, generally describe:

b. Land uses and covertypes on the project site.

Land use or Covertypes	Current Acreage	Acreage After Project Completion	Change (Acres +/-)
• Roads, buildings, and other paved or impervious surfaces			
• Forested			
• Meadows, grasslands or brushlands (non-agricultural, including abandoned agricultural)			
• Agricultural (includes active orchards, field, greenhouse etc.)			
• Surface water features (lakes, ponds, streams, rivers, etc.)			
• Wetlands (freshwater or tidal)			
• Non-vegetated (bare rock, earth or fill)			
• Other Describe: _____ _____			

c. Is the project site presently used by members of the community for public recreation? Yes No
i. If Yes: explain: _____

d. Are there any facilities serving children, the elderly, people with disabilities (e.g., schools, hospitals, licensed day care centers, or group homes) within 1500 feet of the project site? Yes No
If Yes,
i. Identify Facilities:

e. Does the project site contain an existing dam? Yes No
If Yes:
i. Dimensions of the dam and impoundment:

- Dam height: _____ feet
- Dam length: _____ feet
- Surface area: _____ acres
- Volume impounded: _____ gallons OR acre-feet

ii. Dam's existing hazard classification: _____
iii. Provide date and summarize results of last inspection:

f. Has the project site ever been used as a municipal, commercial or industrial solid waste management facility, or does the project site adjoin property which is now, or was at one time, used as a solid waste management facility? Yes No
If Yes:
i. Has the facility been formally closed? Yes No

- If yes, cite sources/documentation: _____

ii. Describe the location of the project site relative to the boundaries of the solid waste management facility:

g. Have hazardous wastes been generated, treated and/or disposed of at the site, or does the project site adjoin property which is now or was at one time used to commercially treat, store and/or dispose of hazardous waste? Yes No
If Yes:
i. Describe waste(s) handled and waste management activities, including approximate time when activities occurred:

h. Potential contamination history. Has there been a reported spill at the proposed project site, or have any remedial actions been conducted at or adjacent to the proposed site? Yes No
If Yes:
i. Is any portion of the site listed on the NYSDEC Spills Incidents database or Environmental Site Remediation database? Check all that apply: Yes No
 Yes – Spills Incidents database Provide DEC ID number(s): _____
 Yes – Environmental Site Remediation database Provide DEC ID number(s): _____
 Neither database
ii. If site has been subject of RCRA corrective activities, describe control measures: _____

iii. Is the project within 2000 feet of any site in the NYSDEC Environmental Site Remediation database? Yes No
If yes, provide DEC ID number(s): _____
iv. If yes to (i), (ii) or (iii) above, describe current status of site(s):

v. Is the project site subject to an institutional control limiting property uses? Yes No

- If yes, DEC site ID number: _____
- Describe the type of institutional control (e.g., deed restriction or easement): _____
- Describe any use limitations: _____
- Describe any engineering controls: _____
- Will the project affect the institutional or engineering controls in place? Yes No
- Explain: _____

E.2. Natural Resources On or Near Project Site

a. What is the average depth to bedrock on the project site? _____ feet

b. Are there bedrock outcroppings on the project site? Yes No
 If Yes, what proportion of the site is comprised of bedrock outcroppings? _____%

c. Predominant soil type(s) present on project site: _____ %
 _____ %
 _____ %

d. What is the average depth to the water table on the project site? Average: _____ feet

e. Drainage status of project site soils: Well Drained: _____ % of site
 Moderately Well Drained: _____ % of site
 Poorly Drained _____ % of site

f. Approximate proportion of proposed action site with slopes: 0-10%: _____ % of site
 10-15%: _____ % of site
 15% or greater: _____ % of site

g. Are there any unique geologic features on the project site? Yes No
 If Yes, describe: _____

h. Surface water features.

i. Does any portion of the project site contain wetlands or other waterbodies (including streams, rivers, ponds or lakes)? Yes No

ii. Do any wetlands or other waterbodies adjoin the project site? Yes No
 If Yes to either *i* or *ii*, continue. If No, skip to E.2.i.

iii. Are any of the wetlands or waterbodies within or adjoining the project site regulated by any federal, state or local agency? Yes No

iv. For each identified regulated wetland and waterbody on the project site, provide the following information:

- Streams: Name _____ Classification _____
- Lakes or Ponds: Name _____ Classification _____
- Wetlands: Name _____ Approximate Size _____
- Wetland No. (if regulated by DEC) _____

v. Are any of the above water bodies listed in the most recent compilation of NYS water quality-impaired waterbodies? Yes No
 If yes, name of impaired water body/bodies and basis for listing as impaired: _____

i. Is the project site in a designated Floodway? Yes No

j. Is the project site in the 100 year Floodplain? Yes No

k. Is the project site in the 500 year Floodplain? Yes No

l. Is the project site located over, or immediately adjoining, a primary, principal or sole source aquifer? Yes No
 If Yes:
 i. Name of aquifer: _____

m. Identify the predominant wildlife species that occupy or use the project site: _____ _____ _____	
n. Does the project site contain a designated significant natural community? <input type="checkbox"/> Yes <input type="checkbox"/> No If Yes: <i>i.</i> Describe the habitat/community (composition, function, and basis for designation): _____ _____ <i>ii.</i> Source(s) of description or evaluation: _____ <i>iii.</i> Extent of community/habitat: <ul style="list-style-type: none"> • Currently: _____ acres • Following completion of project as proposed: _____ acres • Gain or loss (indicate + or -): _____ acres 	
o. Does project site contain any species of plant or animal that is listed by the federal government or NYS as endangered or threatened, or does it contain any areas identified as habitat for an endangered or threatened species? <input type="checkbox"/> Yes <input type="checkbox"/> No	
p. Does the project site contain any species of plant or animal that is listed by NYS as rare, or as a species of special concern? <input type="checkbox"/> Yes <input type="checkbox"/> No	
q. Is the project site or adjoining area currently used for hunting, trapping, fishing or shell fishing? <input type="checkbox"/> Yes <input type="checkbox"/> No If yes, give a brief description of how the proposed action may affect that use: _____ _____	
E.3. Designated Public Resources On or Near Project Site	
a. Is the project site, or any portion of it, located in a designated agricultural district certified pursuant to Agriculture and Markets Law, Article 25-AA, Section 303 and 304? <input type="checkbox"/> Yes <input type="checkbox"/> No If Yes, provide county plus district name/number: _____	
b. Are agricultural lands consisting of highly productive soils present? <input type="checkbox"/> Yes <input type="checkbox"/> No <i>i.</i> If Yes: acreage(s) on project site? _____ <i>ii.</i> Source(s) of soil rating(s): _____	
c. Does the project site contain all or part of, or is it substantially contiguous to, a registered National Natural Landmark? <input type="checkbox"/> Yes <input type="checkbox"/> No If Yes: <i>i.</i> Nature of the natural landmark: <input type="checkbox"/> Biological Community <input type="checkbox"/> Geological Feature <i>ii.</i> Provide brief description of landmark, including values behind designation and approximate size/extent: _____ _____ _____	
d. Is the project site located in or does it adjoin a state listed Critical Environmental Area? <input type="checkbox"/> Yes <input type="checkbox"/> No If Yes: <i>i.</i> CEA name: _____ <i>ii.</i> Basis for designation: _____ <i>iii.</i> Designating agency and date: _____	

e. Does the project site contain, or is it substantially contiguous to, a building, archaeological site, or district which is listed on, or has been nominated by the NYS Board of Historic Preservation for inclusion on, the State or National Register of Historic Places?	<input type="checkbox"/> Yes <input type="checkbox"/> No
If Yes:	
<i>i.</i> Nature of historic/archaeological resource: <input type="checkbox"/> Archaeological Site <input type="checkbox"/> Historic Building or District	
<i>ii.</i> Name: _____	
<i>iii.</i> Brief description of attributes on which listing is based: _____	
f. Is the project site, or any portion of it, located in or adjacent to an area designated as sensitive for archaeological sites on the NY State Historic Preservation Office (SHPO) archaeological site inventory?	<input type="checkbox"/> Yes <input type="checkbox"/> No
g. Have additional archaeological or historic site(s) or resources been identified on the project site?	
If Yes:	
<i>i.</i> Describe possible resource(s): _____	
<i>ii.</i> Basis for identification: _____	
h. Is the project site within five miles of any officially designated and publicly accessible federal, state, or local scenic or aesthetic resource?	<input type="checkbox"/> Yes <input type="checkbox"/> No
If Yes:	
<i>i.</i> Identify resource: _____	
<i>ii.</i> Nature of, or basis for, designation (e.g., established highway overlook, state or local park, state historic trail or scenic byway, etc.): _____	
<i>iii.</i> Distance between project and resource: _____ miles.	
i. Is the project site located within a designated river corridor under the Wild, Scenic and Recreational Rivers Program 6 NYCRR 666?	<input type="checkbox"/> Yes <input type="checkbox"/> No
If Yes:	
<i>i.</i> Identify the name of the river and its designation: _____	
<i>ii.</i> Is the activity consistent with development restrictions contained in 6NYCRR Part 666?	
	<input type="checkbox"/> Yes <input type="checkbox"/> No

F. Additional Information

Attach any additional information which may be needed to clarify your project. [See Attached FEAF Section F, Additional Information.](#)

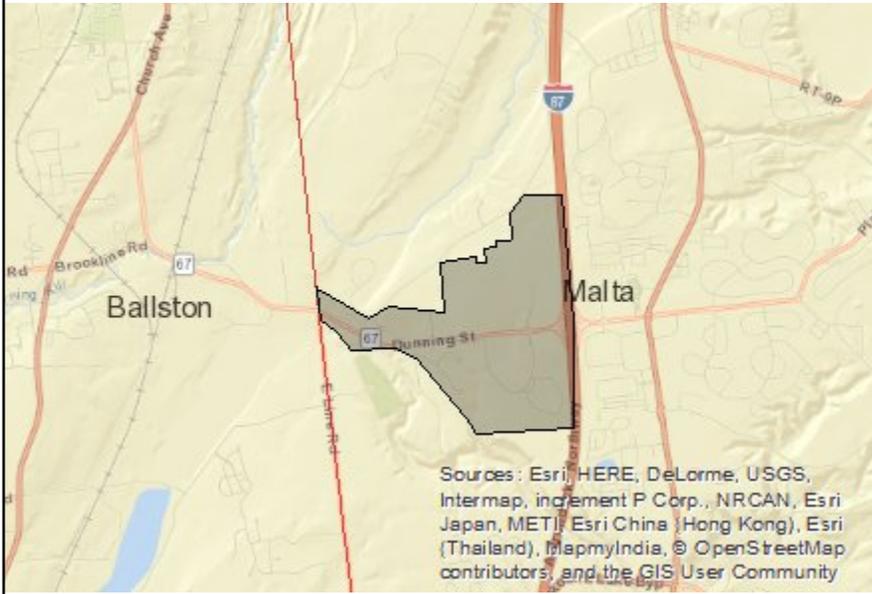
If you have identified any adverse impacts which could be associated with your proposal, please describe those impacts plus any measures which you propose to avoid or minimize them.

G. Verification

I certify that the information provided is true to the best of my knowledge.

Applicant/Sponsor Name _____ Date _____

Signature _____ Title _____



Disclaimer: The EAF Mapper is a screening tool intended to assist project sponsors and reviewing agencies in preparing an environmental assessment form (EAF). Not all questions asked in the EAF are answered by the EAF Mapper. Additional information on any EAF question can be obtained by consulting the EAF Workbooks. Although the EAF Mapper provides the most up-to-date digital data available to DEC, you may also need to contact local or other data sources in order to obtain data not provided by the Mapper. Digital data is not a substitute for agency determinations.



B.i.i [Coastal or Waterfront Area]	No
B.i.ii [Local Waterfront Revitalization Area]	No
C.2.b. [Special Planning District]	Yes - Digital mapping data are not available for all Special Planning Districts. Refer to EAF Workbook.
C.2.b. [Special Planning District - Name]	NYS Heritage Areas: Mohawk Valley Heritage Corridor
E.1.h [DEC Spills or Remediation Site - Potential Contamination History]	Digital mapping data are not available or are incomplete. Refer to EAF Workbook.
E.1.h.i [DEC Spills or Remediation Site - Listed]	Digital mapping data are not available or are incomplete. Refer to EAF Workbook.
E.1.h.i [DEC Spills or Remediation Site - Environmental Site Remediation Database]	Digital mapping data are not available or are incomplete. Refer to EAF Workbook.
E.1.h.iii [Within 2,000' of DEC Remediation Site]	No
E.2.g [Unique Geologic Features]	No
E.2.h.i [Surface Water Features]	Yes
E.2.h.ii [Surface Water Features]	Yes
E.2.h.iii [Surface Water Features]	Yes - Digital mapping information on local and federal wetlands and waterbodies is known to be incomplete. Refer to EAF Workbook.
E.2.h.iv [Surface Water Features - Stream Name]	941-70, 941-123, 941-70.1
E.2.h.iv [Surface Water Features - Stream Classification]	C
E.2.h.iv [Surface Water Features - Lake/Pond Name]	941-70.1
E.2.h.iv [Surface Water Features - Lake/Pond Classification]	C
E.2.h.iv [Surface Water Features - Wetlands Name]	Federal Waters, NYS Wetland

E.2.h.iv [Surface Water Features - Wetlands Size]	NYS Wetland (in acres):28.8
E.2.h.iv [Surface Water Features - DEC Wetlands Number]	R-10
E.2.h.v [Impaired Water Bodies]	No
E.2.i. [Floodway]	Digital mapping data are not available or are incomplete. Refer to EAF Workbook.
E.2.j. [100 Year Floodplain]	Digital mapping data are not available or are incomplete. Refer to EAF Workbook.
E.2.k. [500 Year Floodplain]	Digital mapping data are not available or are incomplete. Refer to EAF Workbook.
E.2.l. [Aquifers]	Yes
E.2.l. [Aquifer Names]	Principal Aquifer
E.2.n. [Natural Communities]	No
E.2.o. [Endangered or Threatened Species]	No
E.2.p. [Rare Plants or Animals]	No
E.3.a. [Agricultural District]	Yes
E.3.a. [Agricultural District]	SARA002
E.3.c. [National Natural Landmark]	No
E.3.d [Critical Environmental Area]	No
E.3.e. [National Register of Historic Places]	Digital mapping data are not available or are incomplete. Refer to EAF Workbook.
E.3.f. [Archeological Sites]	Yes
E.3.i. [Designated River Corridor]	No

**Town of Malta Neighborhood 7
Comprehensive Master Plan Amendment, Rezoning, and New Route 67 West Overlay**

Addendum to FEAF in Accordance with FEAF Part 1 Section F – Additional Information

New York State requires that zoning be adopted in accordance with a community's adopted plan (or comprehensive plan). The most recent version of the Town of Malta's Comprehensive Master Plan was adopted in 2005. The Town completed a Generic Environmental Impact Statement (GEIS) for the 2005 Plan. The Town of Malta is proposing an amendment to the Town of Malta Comprehensive Master Plan adopted in 2005 and associated zoning amendments which include a rezoning of parcels along NYS Route 67 to Office Commercial (C-2) and the creation of a new Route 67 West Overlay District to be applied to these rezoned parcels. Figures 1 and 2 show the location of the project area, and Figures 3 and 4 show Existing Zoning and Proposed Zoning, respectively. Figure 5 shows existing land uses in the vicinity.

The proposed amendment to the Comprehensive Plan and zoning regulations reflect changing conditions of the Town of Malta and the Town's desire to plan for and accommodate growth along the Route 67 corridor while also addressing traffic issues that have grown over the years.

The Town Board appointed a Route 67 West Comprehensive Master Plan Committee on March 21, 2016, to prepare recommendations for zoning and policy changes for the Neighborhood 7 study area. Committee meetings were conducted at Town Hall with members of the public present and participating. Current zoning, planning documents, policies, plans, reports and surrounding land uses and zoning were reviewed. During these meetings, the Committee was provided applicable information from support staff and real estate professionals.

Comments and recommendations from the public, which included comments from a residential real estate professional, a commercial real estate professional, and a commercial broker, included the following:

- Commercial uses could help to improve existing traffic problems.
- Route 67 is a dangerous road.
- Residential development, especially single or two-family, is not marketable/viable given the issues regarding Route 67.
- Although sales of homes along Route 67 have been very poor, sales along Ruhle Road are generally OK.
- Sales in Marvin Woods have been difficult where homes are close to Route 67.
- While Saratoga County has excellent north-south road networks, east-west routes are challenging.
- If zoning is amended to permit commercial uses along Route 67, it should extend at least 300 feet from the highway.
- It is unlikely that any business or number of businesses, such as office or small business, will have the wherewithal to be able to pay for off-site improvements to the existing street network in Neighborhood 7.
- If a very large business (such as a Cabela's) were to site along Route 67, it could have adequate resources to pay for off-site improvements, however the business would also generate a high volume of traffic, which could offset road improvements.
- Traffic problems could potentially scare away some types of small business.
- Noted that a flex-park/distribution center would be viable.
- Not sure if retail will work on Route 67.
- Malta will likely get some single and two-unit types of commercial uses.

- Properties like Double M, Catholic Church site could turn into an industrial park similar to Curtis.
- Office and some retail may be viable.
- There will probably be a need to consolidate parcels in some locations along Route 67 to establish developable parcels.
- Apartments and owner-occupied commercial buildings are the most viable options.

A revised rendition of the Neighborhood 7 text within the Comprehensive Master Plan is proposed. The revised vision and plan for Neighborhood 7 will establish policy to appropriately allow the Town Board to adopt zoning amendments and other related actions in the future.

The Committee identified several elements within the Neighborhood & Comprehensive Master Plan policy which require revision, broader delineation, or new inclusion. The elements that were of highest concern include:

- The need to amend zoning to more appropriately reflect various characteristics of the Route 67 corridor, so that the highest and best use of property may be established in the future;
- Identification of traffic problems that have developed to become of substantially higher concern from what was identified in the 2005 Neighborhood 7 plan; and
- Recognition that the segment of the Route 67 corridor serves as an important gateway given its location between the Northway and Town's western boundary at Eastline Road, and the need to ensure high aesthetic quality evolves within this corridor.

The Committee took special note of adjoining zoning in the Town of Ballston, as well as recent development approvals in Ballston and Ballston Spa, and deliberated how Route 67 in Malta would be impacted. The Committee felt that zoning amendments should be made to simultaneously take advantage of the presently high and growing traffic volume along the corridor by enacting appropriate zoning amendments, but taking care to permit uses which would generally be supported by pass-by traffic or uses that would be less likely to generate significant increases to traffic volume.

An assessment of the recent amendments to C-2 zoning was made and determined that, with appropriate modifications, the C-2 would likely serve to regulate highway commercial development well along Route 67, within Neighborhood 7. Overlay work that is currently under development for the C-2 zone was generally described and understood by the Committee. The C-2 overlay work along Route 9 is targeting to address design standards, architecture, greenspace and buffers.

To tailor C-2 zoning to better correlate zoning regulations to the Route 67 corridor, the Committee recommends that similar overlay provisions be established, and recommends that additional overlay district regulations be developed to address the following issues, and others:

- Eliminate single and two-family dwelling development as permitted uses.
- Establish a minimum 50' building setback from the Route 67 right-of-way.
- Permit parking in front of buildings, provided that such parking is set back a minimum of 50' from the Route 67 right-of-way and provided that a minimum 100' building setback is established.
- Include buffer provisions to protect residential uses.
- Prohibit tree clearing within setback areas where residential property is adjacent to new development, unless otherwise permitted by the Planning Board upon development review.
- Increase maximum lot coverage from 50% to 60% to reduce average development costs in order to promote more cost effective improvements to utility infrastructure.

Development of multifamily residential uses was given consideration by the Committee. The Committee considered the Town Board’s evolving perspective regarding multifamily development; however, there was some difficulty understanding what direction the TB as a body may be taking. Subsequently, the Committee is recommending that multifamily development be permitted, but situated such that it is no closer than 500 feet from the Route 67 right-of-way and this dimensional requirement be included as part of the overlay district regulations.

Committee Chair John Hartzell met with a representative of the Catholic Diocese of Albany to gain an understanding of what future development plans may exist, if any. John learned that the Church does not have immediate plans to develop. As part of the discussion, the Diocese indicated that it would not generally oppose zoning changes which would relate to their property, and that commercial/residential mixed use development would not be objectionable. The Diocese, did, however, wish to preserve its rights to develop its property for church/religious purposes.

Perhaps the largest concern is in regard to traffic problems related to Route 67 and the sense that, despite DOT’s projections of future intersection functionality, traffic is presently problematic and anticipated to become substantially worse as uses in the Town of Ballston and Village of Ballston Spa develop. The highway surface conditions are poor, left turns onto or from Route 67 are difficult and hazardous, traffic volume is already very high, and while the roundabouts improved traffic circulation, the improvements have become detrimental to local residents because there are no 'breaks' in traffic to allow safe turning movements. While adding traffic volume to Route 67 will result in further burden to drivers, if no action is taken to address the under-zoning and subsequent economic impacts to Neighborhood 7 residents, the result will be a continued deterioration of the overall Neighborhood due to traffic impacts, and Malta will not enjoy the benefits of tax base growth, employment opportunities and additional amenities which will result from allowing reasonable commercial development along Route 67.

A traffic volume analysis was performed to provide a comparison of potential traffic based on existing zoning to potential traffic based on the proposed zoning amendments. The analysis considered property size and configuration as well as proposed parking and bulk requirements (refer to Figure 6, Build Potential Map). Based on buildable lots in the proposed area to be rezoned, 11 of the 32 existing lots are considered to have development potential under the existing zoning, resulting in the potential for 63 single family housing units. Due to the required 500-foot front setback from Route 67 with the proposed zoning, only two parcels were evaluated for a mix of commercial and multi-family development, and the remaining parcels were evaluated for commercial use only. Based on this assumption, the potential buildout with the proposed zoning is estimated at 1,236 multifamily housing units, 556,500 SF of commercial space, assuming 3-story buildings with retail on the ground floor (185,500 SF) and office (371,000 SF) on the upper floors.

The following table compares estimated trip generation of potential buildout with existing zoning to potential buildout with proposed zoning for the weekday p.m. peak hour, as described above.

	Existing Zoning	Proposed Zoning	Potential Increase
Enter	45	625	580
Exit	25	800	775
Total:	70	1,425	1,355

The trip estimates for the proposed zoning include an 8% internal trip credit and a 25% pass-by trip credit for the retail use. This volume analysis shows a potential increase of 1,355 vehicle trips during the weekday p.m. peak hour as a result of the proposed zoning amendments at full buildout.

The Saratoga County Regional Traffic Study included an analysis of the Route 67 volumes and capacity thresholds for Existing conditions, Planned Development conditions, and Aggressive Growth conditions. As presented in the study, the Route 67 capacity is 2,000 vehicles per hour (vph) at level of service D (total both directions) and is 2,600 vph at level of service E (total both directions).

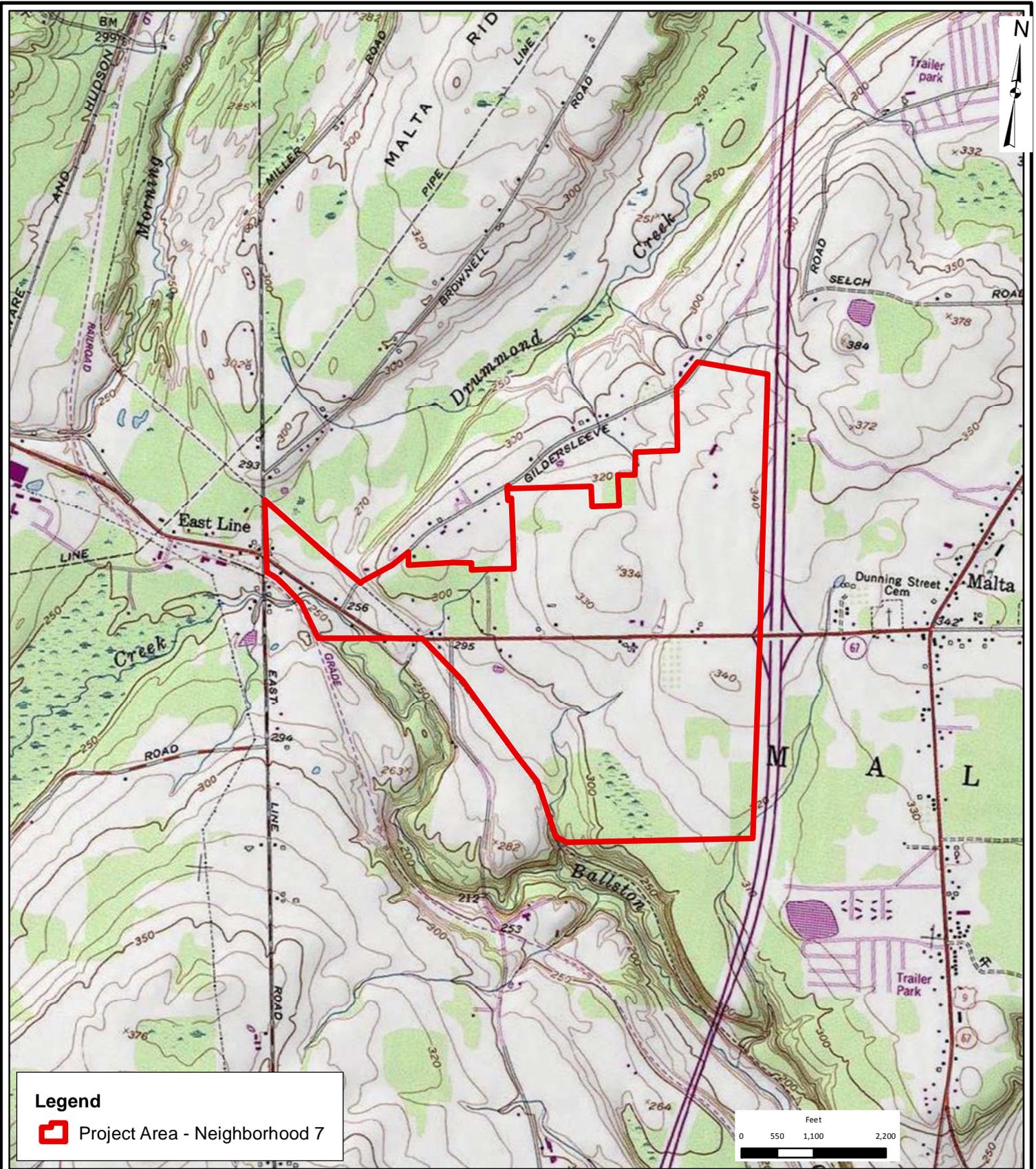
The two-way volume in the study for the projected Aggressive Growth condition is 1,950 vph which is at level of service D conditions. Adding the difference in trips between existing zoning and proposed zoning puts the two-way volume on Route 67 at about 3,300 vph, which exceeds the 2-lane capacity of Route 67. The estimated capacity at level of service E of Route 67 with a flush median (two-way left-turn lane) is 3,250 vph, slightly less than the projected total volume under the proposed zoning condition.

While the proposed zoning does not directly add traffic to Route 67, subsequent development may add a significant amount of traffic, such that improvement to the mainline capacity will be required to accommodate the projections.

The amendments to the Neighborhood 7 section of the Comprehensive Master Plan include recommendations for improvements to Route 67, as well as the future construction of a new two-way street that runs parallel to Route 67 to provide an alternative for local travelers. While the Comprehensive Master Plan will not directly result in actual improvements, establishing a policy that reflects these needs will provide guidance for future evaluations and/or improvements to Route 67 and/or the overall neighborhood street network. The Committee also took special consideration of adjoining zoning in the Town of Ballston, as well as recent development approvals in Ballston and Ballston Spa, and deliberated on the impacts of these on the portion of Route 67 in Malta. The proposed zoning amendments will simultaneously take advantage of the presently high and growing traffic volume along the corridor, while taking care to permit uses which would generally be supported by pass-by traffic or uses that would be less likely to generate significant increases to traffic volume. The proposed zoning revisions include provisions related to access management and internal linkage that will help mitigate traffic flows, including shared access and cross access (Section 3.1 of the Route 67 West Overlay Design Standards & Guidelines). The proposed zoning was prepared and recommended by a committee (with public input) that examined the potential impacts to community character, including traffic impacts.

Furthermore, the Town's existing town-wide GEIS examined anticipated significant town-wide traffic volume increases resulting in the adoption of traffic mitigation fees that address potential impacts that are within the GEIS thresholds. These mitigation fees may be used, in part, for improvements in the Route 67 corridor. The Town is currently updating the GEIS, which may result in additional traffic mitigation at a Town-wide scale. Future development projects will require site-specific SEQR reviews, and any traffic impacts that exceed the thresholds noted in the GEIS will be addressed through specific mitigation measures.

FIGURES



THE Chazen COMPANIES
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 ENVIRONMENTAL & SAFETY PROFESSIONALS
 LANDSCAPE ARCHITECTS

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 21 Fox Street, Poughkeepsie, NY 12601
 Phone: (845) 454-3980

Capital District Office:
 547 River Street, Troy, NY 12180
 Phone: (518) 273-0055

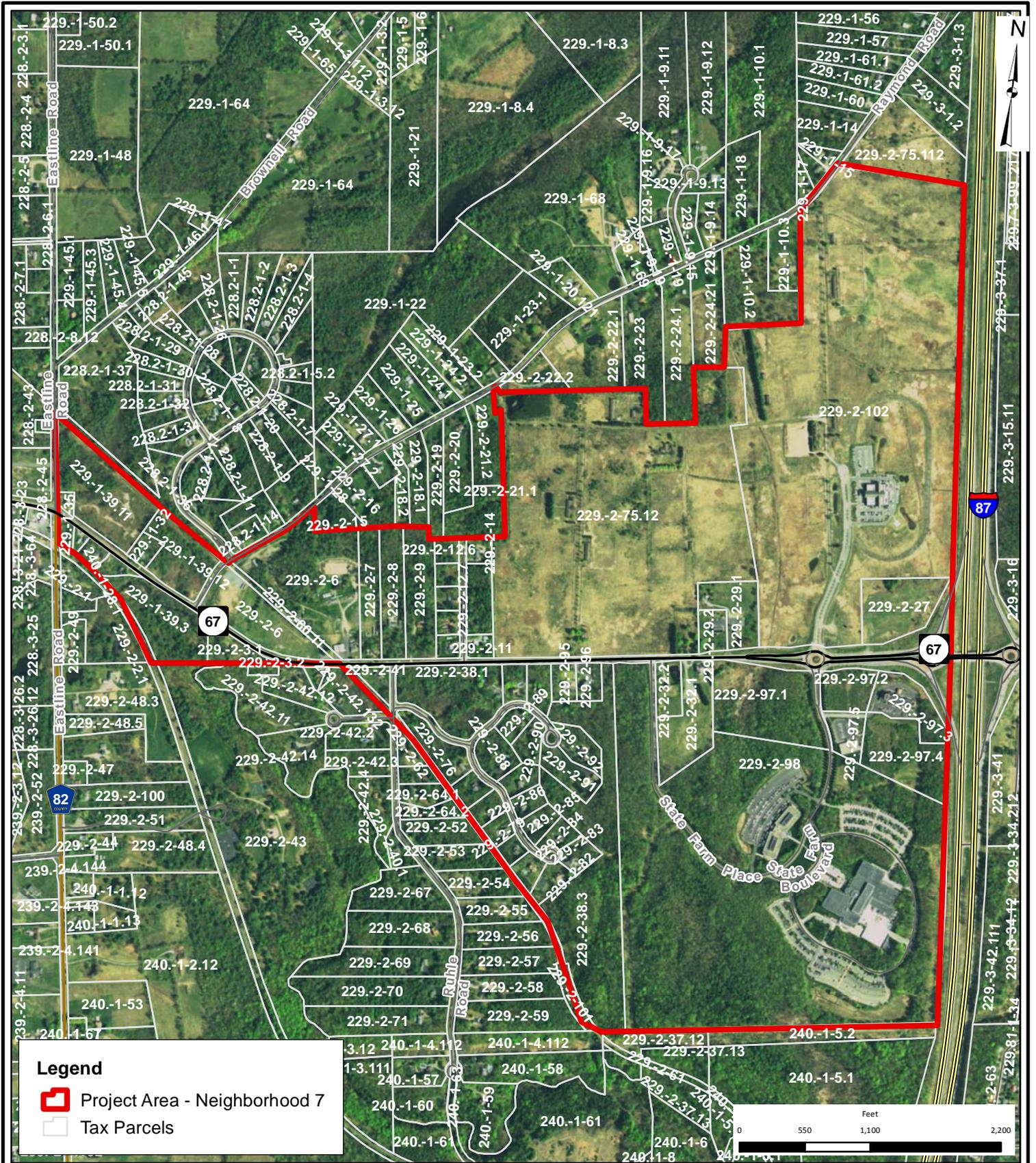
North Country Office:
 375 Bay Road, Queensbury, NY 12804
 Phone: (518) 812-0513

Comprehensive Plan and Zoning Amendments

USGS Location Map

Town of Malta - Saratoga County, New York

Drawn:	RLB
Date:	08/10/2016
Scale:	1 in = 2,000 feet
Project:	81406.00
Figure:	1



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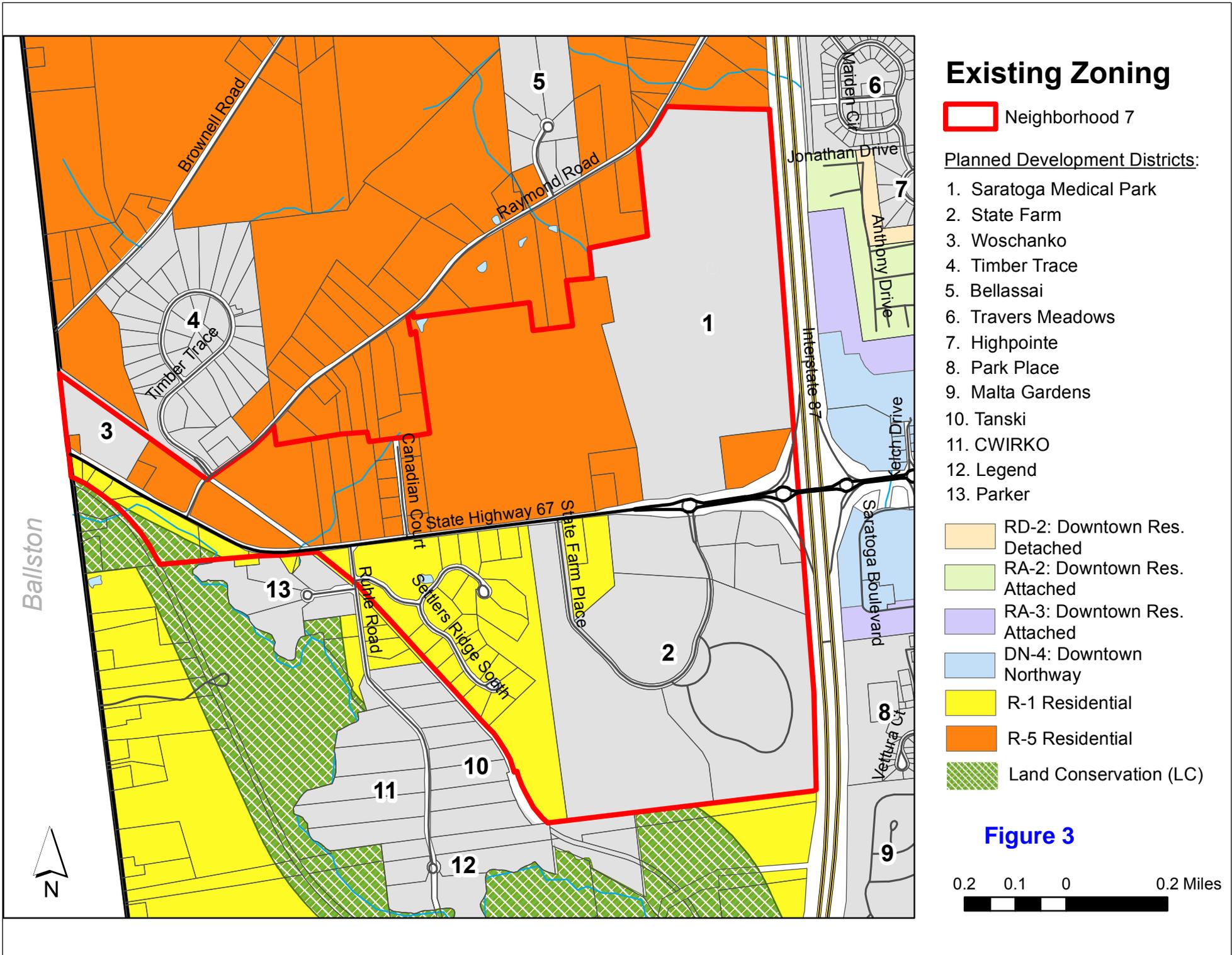
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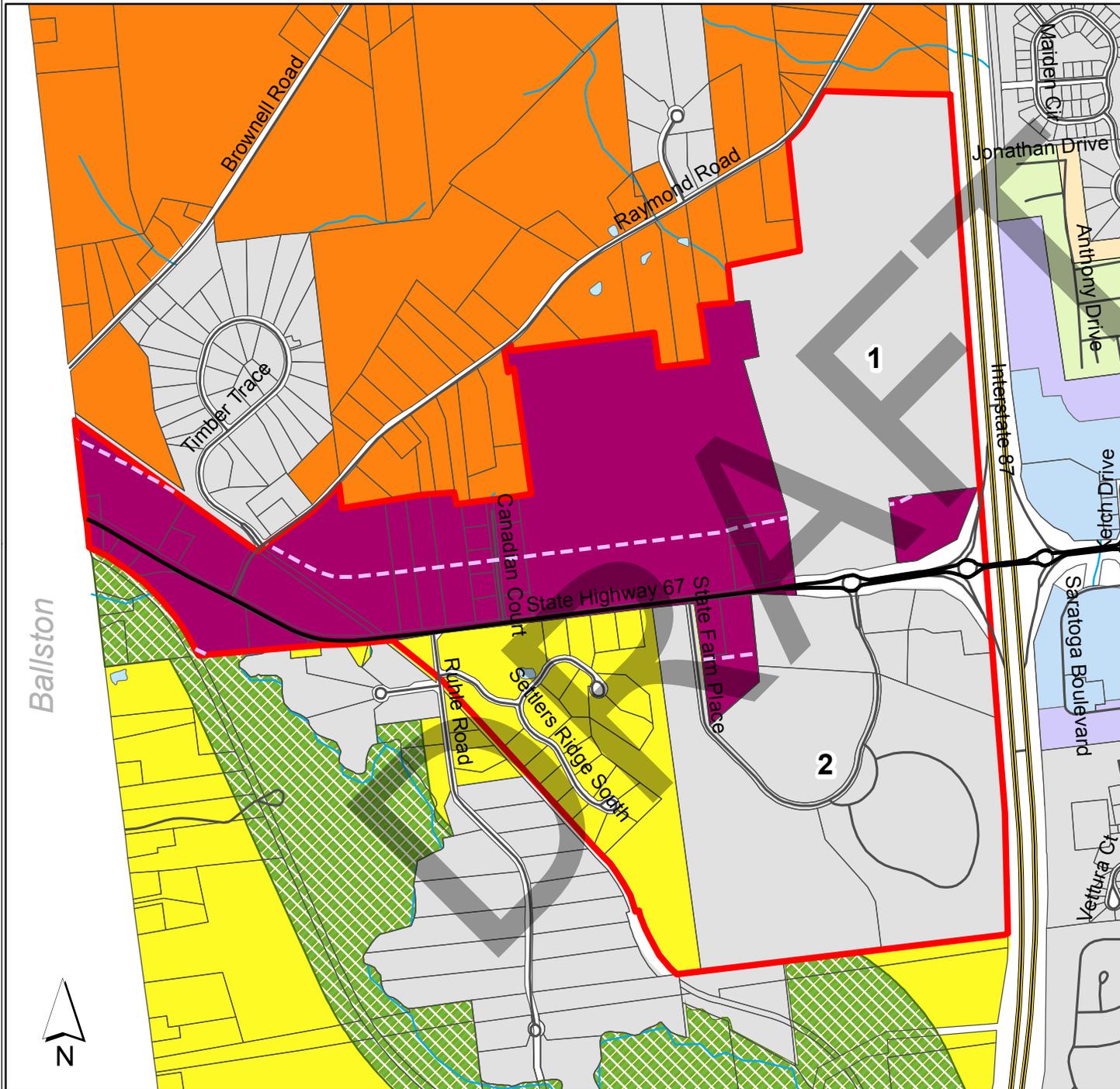
Comprehensive Plan and Zoning Amendments

Orthophoto Tax Map

Town of Malta - Saratoga County, New York

Drawn:	RLB
Date:	08/10/2016
Scale:	1 in = 1,100 feet
Project:	81406.00
Figure:	2

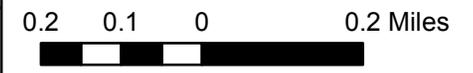


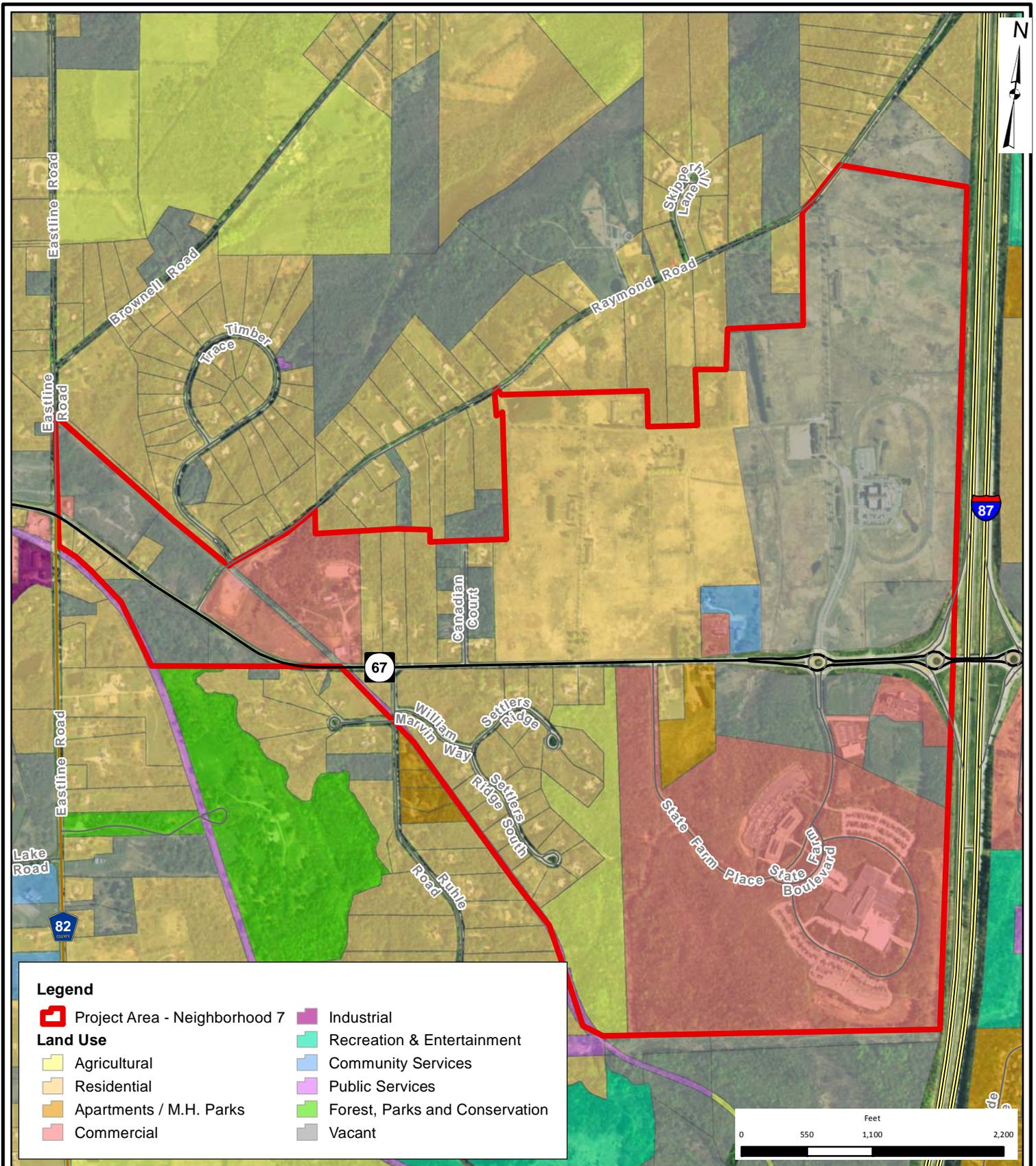


Proposed Zoning

- Neighborhood 7
- Planned Development Districts
 1. Saratoga Medical Park
 2. State Farm
- RD-2: Downtown Res. Detached
- RA-2: Downtown Res. Attached
- RA-3: Downtown Res. Attached
- DN-4: Downtown Northway
- R-1 Residential
- R-5 Residential
- Land Conservation (LC)
- C-2 and Rt. 67 West Overlay (new)
- 500 ft. from Road ROW

Figure 4





Legend

- Project Area - Neighborhood 7
- Industrial
- Agricultural
- Residential
- Apartments / M.H. Parks
- Commercial
- Forest, Parks and Conservation
- Community Services
- Public Services
- Vacant
- Recreation & Entertainment

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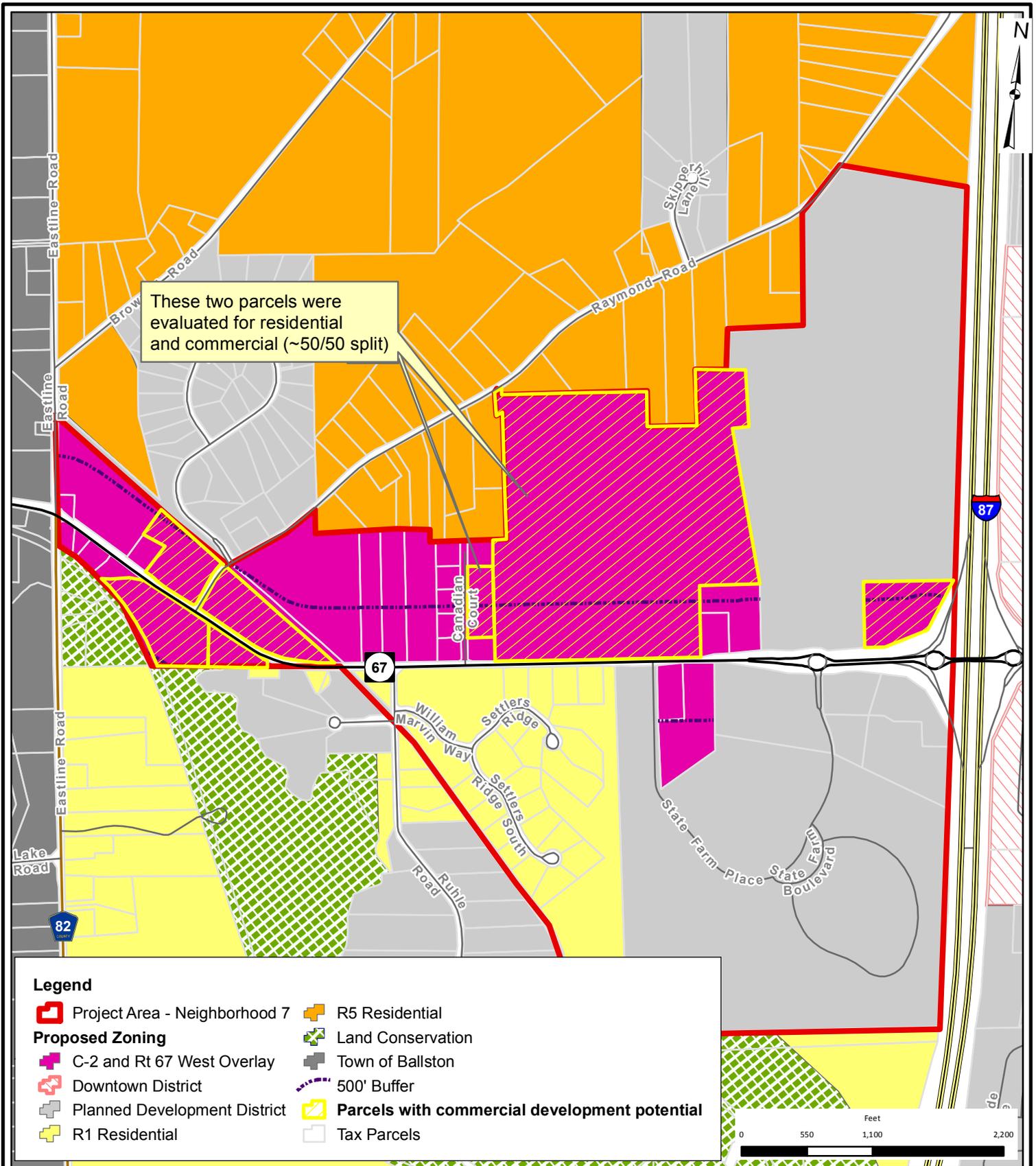
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Comprehensive Plan and Zoning Amendments

Land Use Map

Town of Malta - Saratoga County, New York

Drawn:	RLB
Date:	08/10/2016
Scale:	1 in = 1,100 feet
Project:	81406.00
Figure:	5



These two parcels were evaluated for residential and commercial (~50/50 split)

Legend

- ▭ Project Area - Neighborhood 7
- ▭ R5 Residential
- ▭ Proposed Zoning
- + C-2 and Rt 67 West Overlay
- ▭ Land Conservation
- + Downtown District
- ▭ Town of Ballston
- ▭ Planned Development District
- ▭ 500' Buffer
- ▭ Parcels with commercial development potential
- + R1 Residential
- ▭ Tax Parcels



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Comprehensive Plan and Zoning Amendments

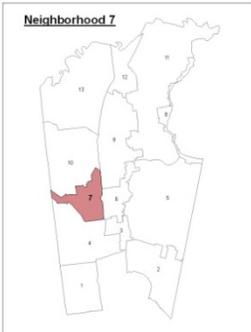
Proposed Zoning Map
Build Potential

Town of Malta - Saratoga County, New York

Drawn:	RLB
Date:	09/09/2016
Scale:	1 in = 1,100 feet
Project:	81406.00
Figure:	6

ATTACHMENT A
Comprehensive Master Plan
Draft Text Amendment

Neighborhood 7 – Route 67, West of Exit 12.....



Except for the Saratoga Medical Park, Double M Western Store and the State Farm Office building at Exit 12, this neighborhood is primarily residential and agricultural. Uses within PDD's in Neighborhood 7 include but are not limited to: medical facilities and dwellings, offices, motel, and restaurant. The areas of Neighborhood 7 that are not zoned as Planned Development Districts are zoned R-1, R-5 or LC.

Since zoning amendments were adopted in 2006, it has become apparent that single family development along a heavily travelled arterial and truck route has not resulted in realistic zoning to achieve the highest and best practical use of property. Existing residents of single family homes fronting onto Route 67 have been unable to sell their properties because there is a low market desire to purchase a residence due to the characteristics along Route 67. As was recognized in the previous rendition of the Town Comprehensive Master Plan for Neighborhood 7, "Malta can expect pressure for commercialization along this corridor". However, despite recognition of the commercial demand, zoning was not amended to appropriately reflect the commercial market demand.

Route 67 serves as an important gateway, both due to its intersection with the Northway, and due to the municipal border edge at its intersection at Eastline Road. It is therefore significant to recognize that these two characteristics, as well as the high volume of traffic along Route 67, establishes an important image of the town of Malta which should continually be improved as opportunities arise to do so.

Route 67 is a state highway and is the heaviest travelled and most important east/west transportation corridor in Malta. With the exception of the I-87 Northway and U.S. Route 9, traffic volume between the Northway and Eastline Road generally has the highest volume of traffic within the Town. It is the primary means of access to our town and I-87 from Ballston Spa, and other communities connected to Route 50, and to the west. A number of traffic studies have been conducted regarding this corridor, including the New York State Department of Transportation, the Saratoga County Regional Traffic Study conducted by the Capital District Transportation Committee, and other various traffic studies relating to private project development proposals. The NYS DOT completed intersection improvements by constructing roundabouts at the intersections of Route 67 and the Northway and the east end of State Farm Road in 2006. Over the past decade traffic volume increase along Route 67 has become a growing concern among residents in Neighborhood 7.

While these 2006 improvements have served to facilitate the continued movement of traffic along Route 67, they have also had detrimental impacts to drivers travelling along other roads that intersect with Route 67. The improved movement of traffic, combined with an increase in traffic volume, has created a situation where drivers must wait excessive times and safety issues have developed when drivers turn onto Route 67, especially when turning left.

Repaving of Route 67 between the Northway and Route 50 has been scheduled since 2001 on the Transportation Improvement Program. Should this work be implemented without significant traffic safety improvements, such improvements are likely to be deferred one or two decades because it would not be cost effective to undertake such safety improvements until future significant improvements to Route 67 pavement is undertaken. The Town should encourage traffic safety improvements as a very high priority, and should encourage such improvements to be implemented at the earliest opportunity possible.

Utility infrastructure within Neighborhood 7 poses challenges toward supporting future development.

Sewer services are provided by Saratoga County Sewer District #1 along the south side of Route 67, between the Northway and Ruhle Road North and the Saratoga Medical Campus. Subsequently, no sewer utilities presently exist along the north side of Route 67 between the Saratoga Medical Campus to Eastline Road, and along the south side of Route 67 between Ruhle Road and Eastline Road.

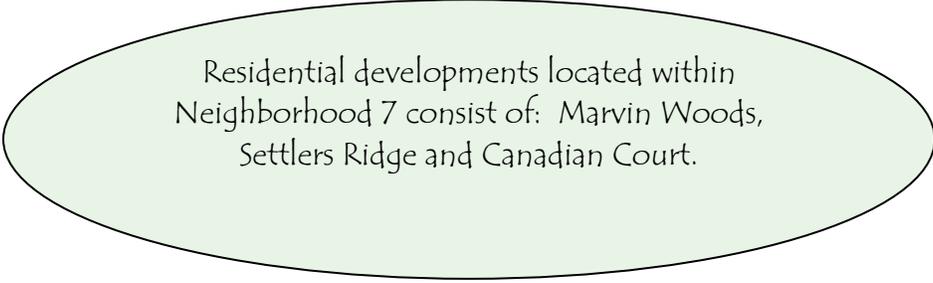
SCSD#1 sewer utilities presently serve State Farm and the Saratoga Medical Campus via a trunk line that extends south of the State Farm property, where it is further extended by a siphon to a larger trunk line that crosses Ballston Creek. SCSD#1 also presently maintains a large trunk line which runs along Eastline Road. It would appear that, to accommodate future development along Route 67, eastern extension of the Eastline Road trunk line to connect to the sewer infrastructure serving State Farm/Saratoga Medical Campus would be required.

Water services are provided by the Clifton Park Water Authority within Neighborhood 7. Some properties along Eastline Road, south of Route 67, are connected to public water provided by CPWA, however these are isolated and are 'out of district' connections served by a water district in the Town of Ballston maintained by CPWA.

CPWA provides water service to the State Farm and the Saratoga Medical Campus. Recent upgrades have been made to improve water service by connection to the County Water Authority trunk line located adjacent to the Zim Smith Trail, in the general vicinity of the southern end of Ruhle Road North. This extension and connection has greatly relieved previous concerns regarding water capacity. While water pressure remains an issue, it would only relate to tall buildings and the issue can be addressed via water pumps to increase pressure at higher building elevations.

To accommodate development along Route 67, a logical approach would appear to be looping the CPWA infrastructure by extending water mains serving State Farm and the Medical Campus along Route 67 to connect to the County Water Authority trunk line near the intersection of Eastline Road and Route 67.

Because these utility improvements will require substantial financial resources, development density along Route 67 should permit lot coverages of 60%, so that a greater concentration of commercial development can contribute to the cost of these utility extensions and make them more financially feasible.



Residential developments located within Neighborhood 7 consist of: Marvin Woods, Settlers Ridge and Canadian Court.

It is the VISION That

The Route 67 corridor consisting of approximately 600 acres is the western gateway to Malta, and as such, introduces visitors to Malta and all that it aspires to be. Malta's goal for this area is to create a positive first impression of Malta. Commercial uses should be encouraged along Route 67 that provide well designed development and attractive architecture. Multifamily residential development should not be located closer than 500 feet from the Route 67 right-of-way and the development of such uses should reflect future Town policies regarding density and distribution throughout the Town. The State Farm and the Saratoga Medical Campus Planned Development Districts each retain the potential for further development.

Safety and other improvements to the transportation network within Neighborhood 7, especially along Route 67, are strongly encouraged to address traffic safety concerns, as well as further safety improvements necessary to accommodate commercial development along the corridor.

A rational strategy must be developed, in consultation with NYS Department of Transportation, Capital District Transportation Committee, Saratoga County Department of Public Works, Town of Malta Highway Department, and other transportation related organizations, to simultaneously improve traffic safety as well as the future transportation impacts that would result in the development of commercial uses along Route 67. Future development north of Route 67 should include a new two-way street situated parallel to Route 67 allowing travel easterly and westerly and, where practical, utilize easements to accommodate such roadway improvements. An emergency access-only shall be provided from Raymond Road onto the parcel presently owned by the Roman Catholic Diocese of Albany.

Zoning should be amended to establish an appropriate highway commercial corridor along Route 67. These amendments should consider utilization of present C-2 zoning, and should include development of overlay zoning to make appropriate modifications from C-2, as well as to address highway access, signage, landscaping, buffers between commercial and residential uses, architectural design, parking, and other related zoning elements that can be specifically tailored to the Route 67 corridor.

This vision also includes safe access within and to adjacent districts for bicyclists and pedestrians; however safety improvements will be needed prior to achieving these objectives. There should be better pedestrian accessibility to connect to the downtown district, as reflected in the Linkage Study. All future improvements to Route 67 should include an off-road multi-purpose trail. Care should be taken, as would be practical, to preserve the natural aesthetic quality of the corridor even as greater development ensues.

.The PLAN -

The initial vision calls for three simultaneous objectives: 1) establishment of highway commercial zoning, 2) promotion of traffic safety improvements along Route 67, and 3) enhancement of improved image and other related aesthetic characteristics along the Route 67 corridor.

Zoning amendments should be adopted to establish commercial zoning along Route 67 by utilizing present C-2 zoning as a general baseline and incorporating overlay zoning to amend uses to tailor and reflect the vision for the Route 67 corridor. Overlay zoning should also be utilized to address elements delineated in the neighborhood 7 vision. In order to provide incentives for slightly denser development than presently provided in the C-2 zoning district, overlay zoning should include provisions to permit up to 60% of lot coverage and minimum front building setback distance of 50' from the Route 67 right-of-

way. Parking should be permitted to be located in front of buildings, however where such development design elements are proposed, such parking should be located no closer than 50' from the Route 67 right-of-way and related front building setbacks should be increased to 100' from the right-of-way. To protect and, to the greatest extent possible, preserve stands of trees, zoning amendments should include prohibition of tree stand clearing or building within setback areas, unless otherwise permitted by the Planning Board during site plan, subdivision and/or special use permit review.

Care should be given so that future established uses do not compete or otherwise conflict with uses established in the core Downtown area (Neighborhood 6, core FBC zones).

Improvements to the transportation network should be sought, and should include safety and capacity improvements to Route 67 and its intersections, as well as improved road network improvements to allow alternative local routes to Route 67 to travel to other areas within Neighborhood 7 or the Town, with improved connectivity to both support improved alternative local routes, emergency accessibility and emergency alternative transportation routing. Such network improvements could include development of service roadways parallel to Route 67 and extension of existing roadways to allow intersection with other existing or future alternative roadways, to allow travel routes to safer intersections along Route 67.

Multifamily residential development is discouraged within 500 feet from the Route 67 right-of-way, but should be permitted further from the corridor, and should reflect future Town policies relating to the concentration, distribution and type of multifamily developments. Single and two-family developments are discouraged for future development in Neighborhood 7.

The western portion of the former horse farm has been purchased by the Catholic Diocese of Albany and a church, cemetery and potentially a school had been established as potential development, however because the PDD has not been developed and has exceeded the PDD sunset deadline, this parcel may also be a significant opportunity for new development. Zoning should be amended, to include this subject parcel, so that potential development will be complementary to other adjoining developments and existing or potential developments along Route 67 within Neighborhood 7. Consideration should be made to permit uses previously allowed within the subject PDD, as well as additionally permitting present C-2 zoning district uses, and mixed use development which may include multifamily uses.

The Route 67 corridor has the potential to attract one or two developments which may be considered unique and/or special to the overall community. Such special developments may require special dimensional specifications. In order to accommodate such development, the Overlay Zone should include performance zoning which would allow building height of up to 6 stories, via issuance of a special use permit.

The improvements to Route 67 and I-87 Exit 12 interchange provide a significant opportunity for business expansion at the State Farm property. The plan for the build out of the State Farm property is based on the approved PDD, the location of the property, and the availability of utilities. During the past several years additional development has occurred and has included a hotel/conference center, and a building used for backup security of off-site digital records. Three additional office buildings remain to be built pursuant to the State Farm PDD. This site will continue to have little visibility from I-87 or Route 67. Multi-tiered parking garages will be encouraged within the State Farm Insurance complex. Impacts related to the Route 67 plan include increases in traffic from the build out of the State Farm PDD.

As appropriate and significantly needed traffic safety improvements are made to Route 67, projects in this district should include pedestrian links to adjacent parcels and projects, as well as to other districts whenever possible. Pedestrian and bike path connections to the downtown, Ballston Spa, the Zim Smith Trail, and trolley beds and related trails are encouraged.

Image is critical for the entire district. Route 67 corridor development guidelines should include access management, lighting standards, landscaping, and architectural standards, via the use of comprehensive zoning regulations and/or the establishment of overlay zoning districts.

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ATTACHMENT B

Proposed Zoning Amendments

Local Law Revising Area Zoned C-2

1. Change name of §167-19 to “C-2”
2. Revise Town Zoning Map to include area studied by the Malta Route 67 West Comprehensive Plan Update Committee
3. Establish overlay district¹ to cover area studied by Malta Route 67 West Comprehensive Plan Update Committee (supplemented by adoption of Use, Area and Bulk Table)
 - a. Additional Uses Permitted:
 - i. Medical Center;
 - ii. Apartment House;
 - iii. Personal Service Shop;
 - iv. Recreation Facility;
 - v. Restaurant (no drive-thru);
 - vi. Retail; mid-size;
 - vii. Technology/Research Office;
 - viii. Wholesale Business.
 - b. Additional Special Permitted Uses:
 - i. Aquatics Center;
 - ii. Club/Lodge;
 - iii. Convenience Store, with or without gas;
 - iv. Drive-through Accessory Use;
 - v. Gasoline Service;
 - vi. Hotel/Motel;
 - vii. Office Building exceeding maximum building height.
 - c. Uses *not* Permitted in Overlay District:
 - i. Single-Family Homes;
 - ii. Two-Family Homes.
4. Amend §167-2 (Definitions) to include the definition of “Aquatics Center” as: “An establishment composed mostly of indoor recreational and competitive swimming, therapy and diving pools, classroom spaces, aquatic training spaces, locker rooms, restrooms, exercise/weight areas, business offices, meeting rooms, outdoor spray pools, and spectator seating. An Aquatic Center may operate as such despite exclusion of one or more of these elements. Up to 20% of the gross floor area of the principal building shall also be permitted as accessory uses, such as food services, medical offices, retail shops and uses not otherwise specifically prohibited. Food service uses shall be open to the public to provide for the counter sale of food and beverages. Retail shop use shall be open to the public for the sale of merchandise and equipment for aquatic activities. Prohibited uses include drive-through and other uses otherwise identified pursuant to any provision of Town Code. For the purposes of site orientation, the front setback area shall be the distance between the front building elevation and the Route 67 right-of-way property line; if such a use is not developed along Route 67, front setback shall be as defined in general Town Zoning Code.”

¹ Where any conflict between the C-2 zoning and the Route 67 West Overlay exist above, the Route 67 West Overlay criteria shall apply.

ATTACHMENT C
Proposed Use and Bulk Regulations
for C-2 and Route 67 West Overlay

District	Permitted Principal Use	Special Use Required for	Min. Lot Size (s.f.)	** Max. % of Lot Coverage (includes parking)	Min. Frontage Width (ft.)	*** Max. Building Height (ft.)	Front Yard (ft.)	Side Yard (ft.)	Rear Yard (ft.)	Min. floor area (sf)	Max. footprint (sf)
C-2											
	One Family Dwelling		40,000	N/A	200	30	50	15	30		
	Two Family Dwelling		40,000	N/A	200	30	50	15	30		
	Antique shop		40,000	50	200	30	50	20	40		
	Automotive repair		40,000	50	200	30	50	20	40		
	Auto sales and service		80,000	50	200	30	50	30	40		25,000
	Bed & Breakfast, Commercial		40,000	50	200	30	50	20	40		
	Boat & RV Storage		80,000	50	200	30	50	20	40		
	Brew Pub		80,000	50	200	30	50	20	40		30,000
	Business Office		40,000	50	200	40	50	20	40		
	Commercial kitchen		40,000	50	200	30	50	20	40		
	Day Care Center		40,000	50	200	30	50	20	40		
	Fitness Center		40,000	50	200	30	50	20	40		
	Funeral home		40,000	50	200	30	50	20	40		
	Gallery		40,000	50	200	30	50	20	40		
	Grocery store		80,000	50	200	30	50	20	40	10,000	40,000
	Household furnishings		40,000	50	200	30	50	20	40	10,000	35,000
	Library		40,000	50	200	40	50	20	40		
	Media production		40,000	50	200	40	50	20	40		
	Municipal Building		40,000	50	200	40	50	20	40		
	Museum		40,000	50	200	40	50	20	40		
	Nursing or Convalescent Home/Assisted Living		80,000	50	200	40	50	20	40		
	Pet Grooming		40,000	50	200	30	50	20	40		
	Printing and Shipping		40,000	50	200	30	50	20	40		15,000
	Professional Office		40,000	50	200	40	50	20	40		
	Religious worship		160,000	50	200	40	50	20	40		
	School/College/Educational Facility		80,000	50	200	40	50	20	40		
	Theater		40,000	50	200	30	50	20	40		
	Amusement facility		80,000	50	200	30	50	30	50		40,000
	Animal Hospital		40,000	50	200	30	50	20	40		
	Building Materials Sales		40,000	50	200	30	50	20	40	10,000	80,000
	Car Wash		40,000	50	200	30	50	20	40		
	Contractor's Yard		40,000	50	200	30	50	30	50		10,000
	Garden Center/Nursery		80,000	50	200	30	50	20	40		30,000
	Golf Driving Range		80,000	N/A	200	30	50	40	40		
	Laboratory		80,000	50	300	40	50	20	40		
	Light Industry		40,000	50	200	30	50	20	40		
	Medical Center		80,000	50	300	40	50	20	40		
	Microbrewery		80,000	50	200	30	50	20	40		
	Microdistillery		80,000	50	200	30	50	20	40		
	Mixed Use/most restrictive*	*multi-tenant building max. of 3 first floor units, second story habitable space required.									
	Pet Daycare		80,000	50	200	30	50	30	50		
	Recreation Facility		80,000	50	200	30	50	20	40		35,000
	Restaurant (no drive-thru)		40,000	50	200	30	50	20	40		20,000
	Retail, mid-size		80,000	50	300	30	50	30	50	10,000	35,000
	Self Service Storage Facility		80,000	50	200	30	50	20	40		
	Technology/Research Office		80,000	50	200	40	50	20	40		
	Wholesale Business		80,000	50	200	30	50	20	40		40,000

District	Permitted Principal Use	Special Use Required for	Min. Lot Size (s.f.)	Max. % of Lot Coverage (includes parking)	Min. Frontage Width (ft.)	Max. Building Height (ft.)	Front Yard (ft.)	Side Yard (ft.)	Rear Yard (ft.)	Min. floor area (sf)	Max. footprint (sf)	
Route 67 West Overlay *	Medical Center		80000	60	300	50	50	20	40			
	Apartment House		80,000	60	200	40	500	20	40			
	Personal Service Shop		40000	60	200	30	50	20	40			
	Recreation Facility		80000	60	200	30	50	20	40		35000	
	Restaurant (no drive-thru)		40000	60	200	30	50	20	40		20000	
	Retail, mid-size		80000	60	300	30	50	30	50	10000	35000	
	Technology/Research Office		80000	60	200	40	50	20	40			
	Wholesale Business		80000	60	200	30	50	20	40		40000	
		Aquatics Center		200000	50	300	60	50	30	50	10000	65000
		Club/Lodge		80000	60	200	50	50	20	40		
		Convenience Store, with or		40000	60	200	25	50	40	40		
		Drive-thru Accessory Use	Most Restrictive Relative To Permitted Principal Use									
	Gasoline Service		40000	60	200	30	50	20	40			
	Hotel/Motel		80,000	60	200	40	40	20	40	15,000		
	*** Office building exceeding maximum building height		200000	N/A	200	5 Storys	75	50	50		90,000	

Route 67 West Overlay *	PROHIBITED Principal Use
	One Family Dwelling
	Two Family Dwelling

* Where any conflict between C-2 zoning and the Route 67 West Overlay exist above, the Route 67 West Overlay criteria shall apply

** Where C-2 zoning requires a maximum lot coverage of 50%, a maximum lot coverage of 60% shall be required withing the Overlay District

Definitions:

*** Office Building exceeding maximum building height. Any Office Building within the Route 67 West Overlay District which meets all criteria required for issuance of a special use permit, meets the criteria established for Transect Building Heights, and which also meets additional criteria to be considered by the Planning Board that the specific characteristics of the use is demonstrated to be unique and of special value to the community, that the building is of above average architectural design, character, placement, mass and scale, and that the additional building height either supports these special use permit review criteria or are otherwise balanced by the unique benefits to be gained by the overall general community. Any such approved use shall also comply with related regulations within the Route 67 West Overlay Design Standards.

**** Refer to the Route 67 West Overlay Standards for alternative building heights, building height bonus regulations, and building height transect table

ATTACHMENT D
Proposed Route 67 West Overlay District
Design Standards & Guidelines



Route 67 West Overlay District Design Standards & Guidelines

Town of Malta

ADOPTED: September , 2016

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Section 1. General

1.1 Purpose

The Town has established a Route 67 West overlay district that generally correlates to the C-2 zoning district along the Rt. 67 West corridor. In order to ensure that future commercial development in this overlay district is built to a quality design standard, this set of standards and guidelines was developed.

This Standard is intended to provide developers and the town with clear requirements for the design of new and redeveloped commercial projects.

Standards should function as the minimum requirement that an element of site design should meet.

Planning staff has recommended and the Town Board has adopted standards that address the following elements of site plan design:

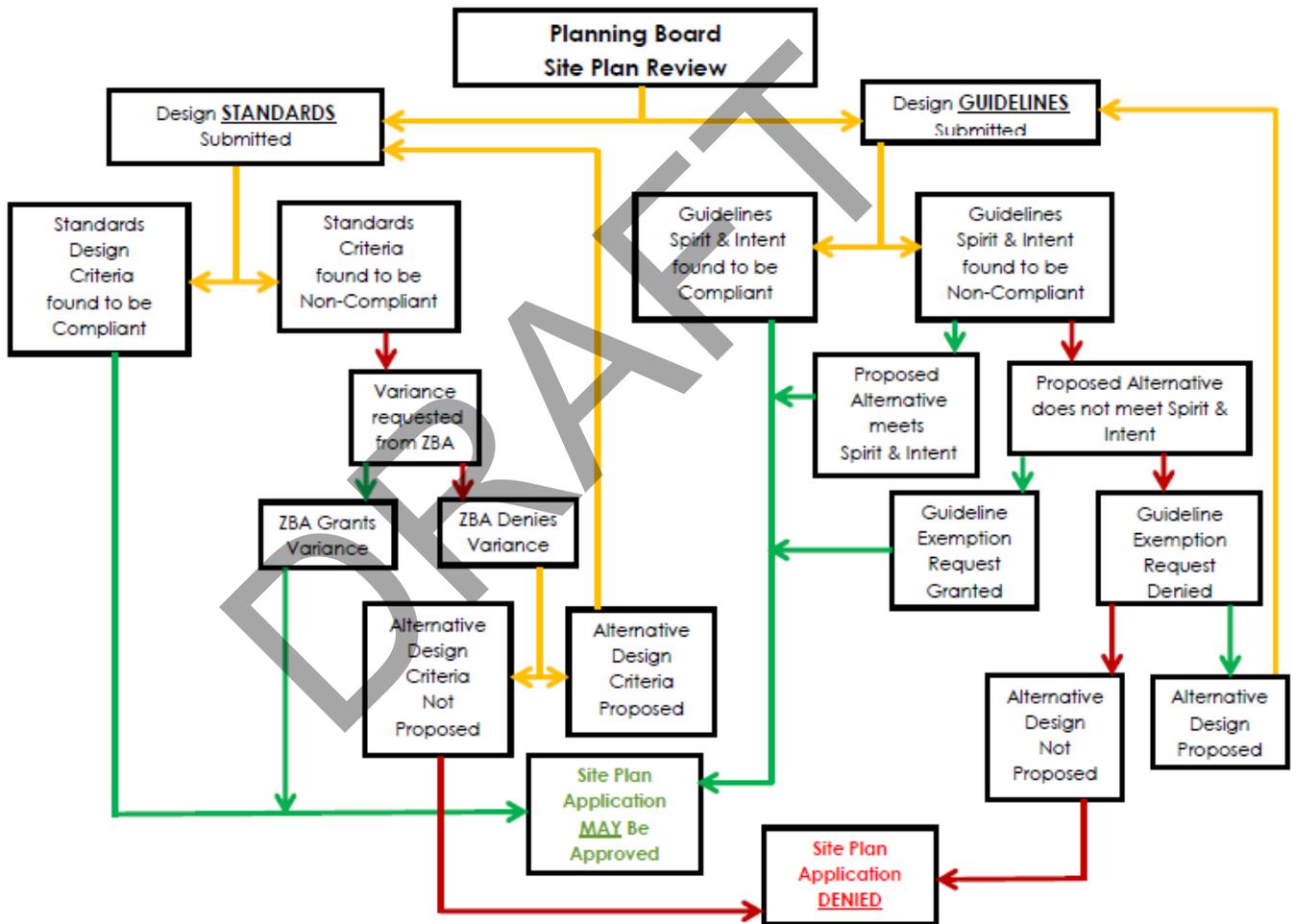
- Architecture
- Access
- Site layout
- Parking
- Lighting
- Landscaping
- Signs
- Greenbelt and buffers
- Outdoor site elements and special uses
- Building Height Transect and Height Bonus

1.2 Applicability

- A. This Standard is applicable to all commercial development in the Route 67 West overlay district.
- B. All references to the “Standard” or “Guideline” refer to this Route 67 West Overlay District Design Standards and Guidelines unless otherwise stated. Unless otherwise noted a Standard can be identified by the use of “shall or must” to describe the action taken and a Guideline can be identified by the use of “should or may.” If it is unclear whether something is a Standard or a Guideline then the Building & Planning Coordinator will determine how the Standard is to be applied. Where design elements are considered to be a Standard, it shall be fully met, unless a variance is granted by the Zoning Board of Appeals. Where design elements are considered to be a Guideline, such elements shall also be fully met; however, if the applicant for a project is able to demonstrate to the Planning Board’s satisfaction that a Guideline cannot be met, due to practical constraints and circumstances beyond the applicant’s control, the Planning Board may choose to require that it be met in a different way, in a lesser degree, or not at all. If the applicant for a project cannot meet a Standard then a variance must be sought and received from the Zoning Board of Appeals prior to Site Plan review.

- C. Where the provisions of these Standards are in conflict with other provisions of Town Code, the regulation which results in the higher standard as determined by the Building and Planning Coordinator shall apply. If the Building and Planning Coordinator determines that a higher regulation cannot be determined, the more recent provision of Town Code shall apply.
- D. Any alterations to legal nonconforming parcels must result in closer conformance to the C-2 zoning and to these Standards.
- E. All allowed uses shall be considered to be commercial uses for the purposes of these Standards and Guidelines and for the purposes of calculating applicable fees in lieu of mitigation as related to the Town-Wide Generic Environmental Impact Statement.

Route 67 West Overlay Zoning Flowchart



Section 2. Buildings

2.1 Setbacks

The zoning district table associated with Town Code Chapter 167 Zoning provides for building setback requirements by district and use. Setbacks may alter from those cited within the zoning district table as a result of implementation of the regulations within these Design Standards. Where conflicts exist, the regulations of the Design Standards shall apply.

2.2 Transparency

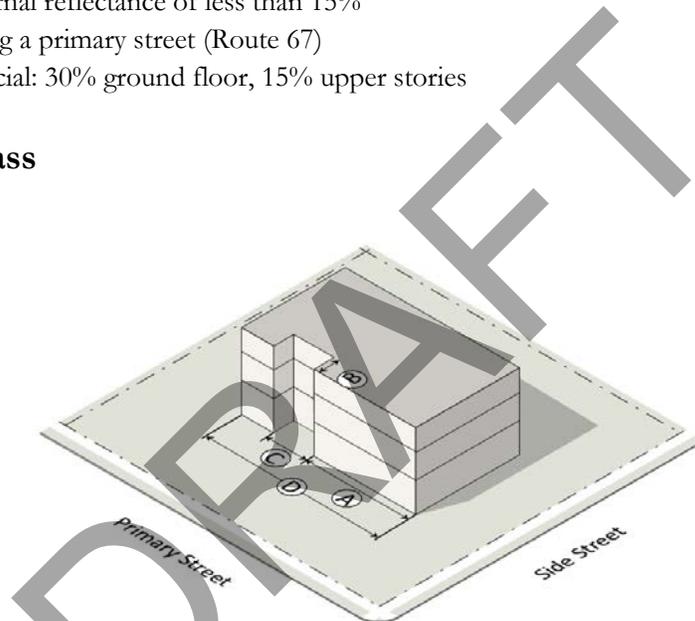
A. Definition

The minimum percentage of windows and doors that shall cover a façade (ground story or upper story) measured between 2 and 12 feet above the adjacent sidewalk. Glass shall have a transparency higher than 80% with an external reflectance of less than 15%

B. Requirement along a primary street (Route 67)

1. Commercial: 30% ground floor, 15% upper stories

2.3 Building Mass



- A. Street-facing wall length without offset maximum: 100'
- B. Street-facing wall offset depth minimum: 4'
- C. Street-facing wall offset length minimum: 8'
- D. Building length, street-facing façade maximum: 300.'

2.4 Pedestrian Access

A. Definition

Defined as a street-facing entrance providing both ingress and egress operable to resident or customers at all times. Additional entrances are also permitted.

- B. Pedestrian access should be provided every 100' along the street-facing façade

2.5 Building Materials

The following materials shall be utilized for all buildings.

A. Primary:

- a. Brick and tile masonry
- b. Cementitious siding
- c. Glass curtain wall
- d. Native stone or synthetic equipment
- e. Stucco (cementitious finish)
- f. Wood- clapboard or shingles
- g. PVC siding - with traditional dimensions only
- h. Other primary materials determined by the Planning Board to be superior to the materials cited above, or which the Planning Board determines are appropriate due to special architectural features determined desirable by the Planning Board, may be substituted from materials cited above.

B. Accent:

- a. EIFS (External Insulation Finishing System)- trim and cornice elements only
- b. Glass block
- c. GFRC (Gypsum Reinforced Fiber Concrete)- trim elements only
- d. Metal- beams lintels, trim elements and ornamentation
- e. Pre-cast masonry- trim and cornice elements only
- f. Split-faced block- piers, foundation walls and chimneys only
- g. PVC trim - with traditional dimensions and shapes only
- h. Other accent materials determined by the Planning Board to be superior to the materials cited above, or which the Planning Board determines are appropriate due to special architectural features determined desirable by the Planning Board, may be substituted from materials cited above.

C. Color

- a. All colors on a building must be of a natural, muted shade.
- b. Other colors determined by the Planning Board to be appropriate due to special architectural features determined desirable by the Planning Board.

2.6 Screening

A. Service Areas

1. Trash collection, trash compaction, recycling collection and other similar service areas must be located to the side or rear of buildings and must be screened from view from adjacent property or public street right-of-way.
2. Service areas that are fully integrated into a building must be screened with a roll down door or other opaque screen.
3. Service areas that are not integrated into a building must be screened on three sides by a wall at least six feet in height and on the fourth side by a solid gate at least six feet in height.

B. Roof-Mounted Equipment

1. Roof-mounted equipment must be screened from ground level view from adjacent property or adjacent public street right-of-way.

2. New buildings must provide a parapet wall or other architectural element that screens roof-mounted equipment from ground level view.

C. Wall-Mounted Equipment

1. Wall-mounted equipment cannot be located on any surface that directly faces a public right-of-way.
2. Wall-mounted equipment located on any surface that is visible from a public right-of-way must be fully screened by landscaping or an opaque screen.

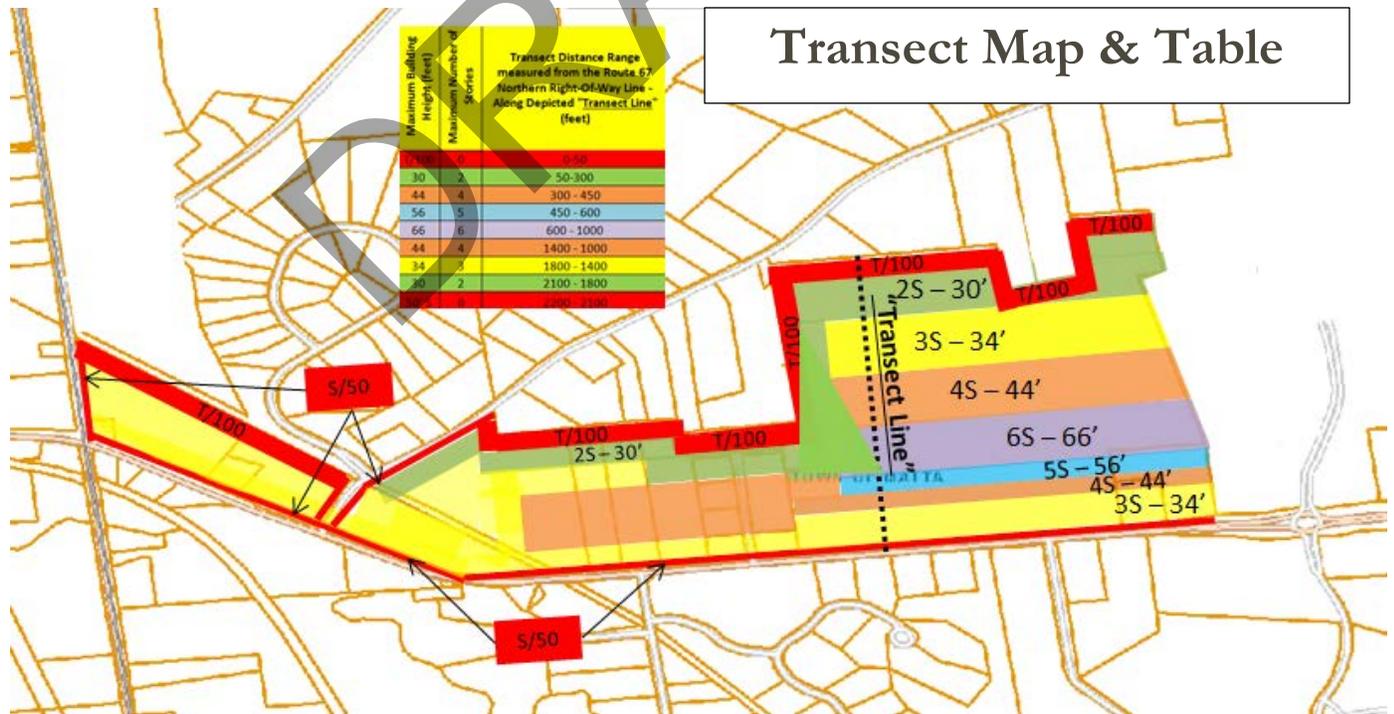
D. Ground-Mounted Equipment

Ground-mounted mechanical equipment that is visible from a public right-of-way must be screened from view by landscaping or a fence or wall. The screening must be of a height equal to or greater than the height of the mechanical equipment being screened.

2.7 Building Height Transect and Height Bonus

A. Building Height Transect

1. The following building height transect shall apply where new development will occur on any parcel located north of Route 67 within the Overlay District. These requirements shall not apply to the parcels located near the northwest corner of the intersection of Interstate 87 & Route 67, identified by tax map number 229.-2-27 or any subdivision or lease lot thereof, or to any parcels south of Route 67 within the Overlay District.



B. Bonus Building Height

1. Where application is made for issuance of a special use permit to allow a building height bonus for an office building, where total building height exceeds multi-story office buildings building height regulations cited within either C-2 zoning or within Route 67 West Overlay District 'Use, Area and Bulk Tables', additional building height may be permitted upon issuance of a special use permit as regulated by the Town Zoning Code and in conformance with the additional special use permit criteria cited herein. Such requests for bonus building height should also comply with the "Transect Map & Table", as delineated within Section 2.7
2. Any office building which exceeds the maximum height established in the 'Use, Area and Bulk Tables' cited above shall provide an additional green space above that area which would be required if the building did not exceed such Tables. The additional green space area shall be calculated as that percentage of gross floor area which exceeds the gross floor area at or below the building height which meets the criteria in the 'Use, Area and Bulk Tables', plus 25% for one additional story, 20% for two additional stories, 15% for three additional stories, 10% for 4 additional stories or more. For example, if an office building having a footprint of 100' by 100' which is permitted a building height of three stories pursuant to the 'Use, Area and Bulk Tables', but which is permitted an additional story 100' by 100', and if the 'Use Area and Bulk Tables' require a maximum lot coverage of 60%, the maximum lot coverage shall be calculated as: $(60)/(100,000/300,000)/0.25 = 45.05\%$; subsequently greenspace is increased by a correlating reduction of maximum lot coverage from 60% to 45.05%.

Section 3. Sites

3.1 Access

A. Site Access

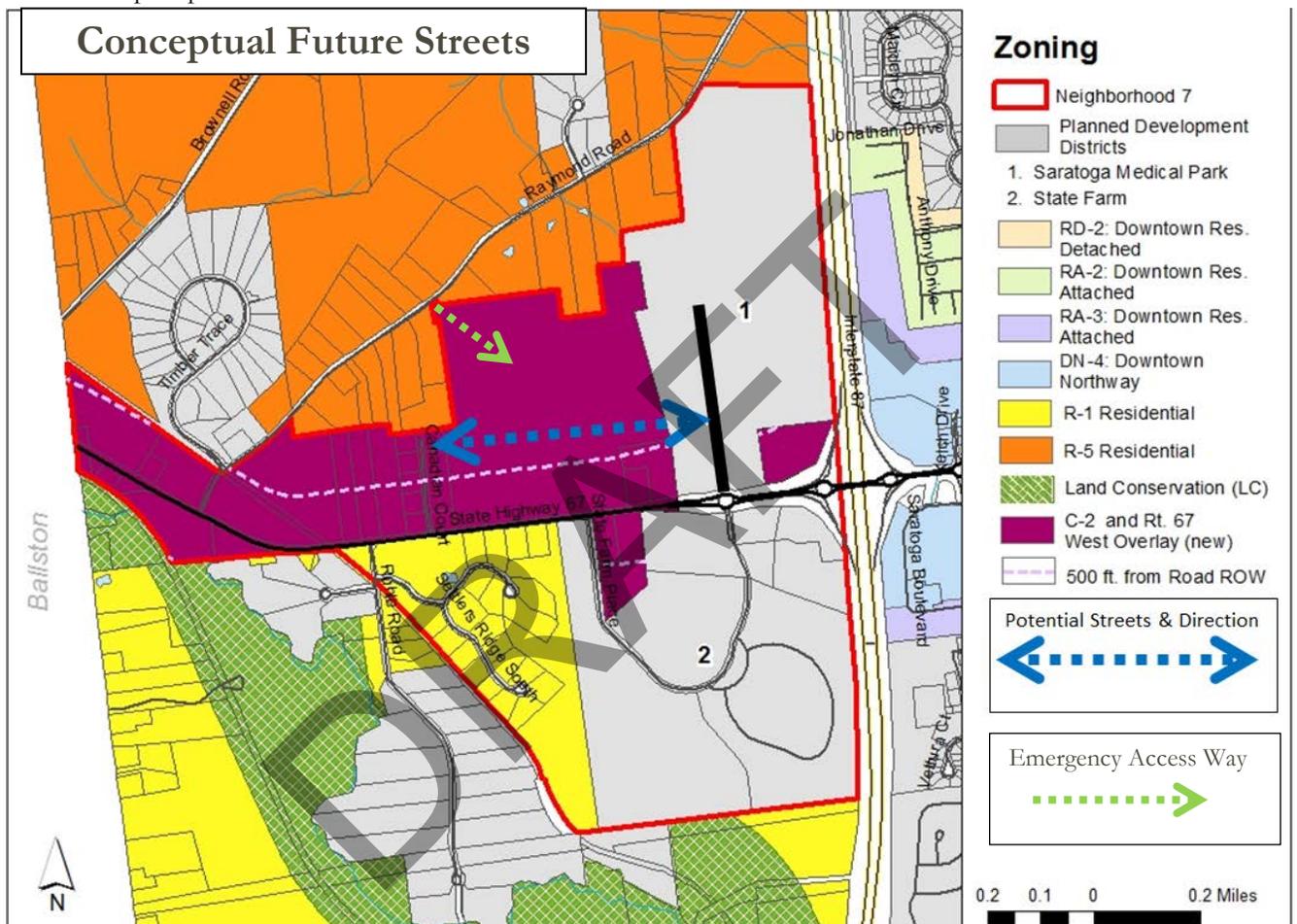
1. Site entry points from Route 67 shall be emphasized as "gateways" for larger scale developments. These entries must be designed as attractive landscaped features that incorporate an integrated set of signage, lighting and planting elements.
2. Continuous service roads towards the rear of the property should be required for vehicular traffic. Continuous pedestrian connections should be built at the front of buildings.
3. Driveways which cross existing sidewalks or where new sidewalks are planned as part of the new development shall have a striped crosswalk and shall provide a connection on either side to the sidewalk. The design should be in conformance with MUTCD and approved by the Town Engineer.

B. Shared Access

1. When adjacent parcels are developed, the owner and Planning Board shall review whether a shared access is feasible and whether project design can accommodate shared access. It is the desire of the Town of Malta that access along Route 67 be shared among new developments.
2. Single owner projects that span multiple parcels should utilize only one access on Route 67. Exceptions to this may be considered by the Planning Board if recommended by Town Planner or Engineer.
3. Where multiple structures and uses are proposed, buildings shall be clustered with access provided by common entrances and internal road systems.

C. Cross Access

1. All development within the areas depicted on the Transect Map & Table lying north of Route 67, south/east of Raymond Road and west of the Saratoga Medical Campus Planned Development District shall be designed for two-way traffic travelling east and west along a roadway generally parallel to Route 67. An emergency access way shall also be provided with access from Raymond Road to existing tax parcel 229.-2-75.12, or any subdivision parcels or lease lots thereof. Actual road design shall be more clearly delineated within a future development proposal or by adoption of policy or code by the Town Board. These designs should generally reflect the ‘Conceptual Future Streets Map’ depicted below:



2. Cross-access for vehicles should be provided between abutting lots along Route 67.
3. A stub for future vehicular cross-access should be provided to all abutting vacant land.
4. When vehicular cross-access is deemed impractical by the Planning Board on the basis of topography, the presence of natural features, vehicular safety factors or incompatible uses, the requirement for cross access may be waived by the Planning Board.
5. If cross-access is not provided at the time of original site plan approval due to the aforementioned conditions in item (3), cross-access should be reconsidered for the site at the time of a change in tenancy or site plan amendment.
6. Property owners who establish cross-access easements shall:
 - a. Record an easement allowing cross-access to and from properties served by the cross access easement.
 - b. Record a joint maintenance agreement defining the maintenance responsibilities of each property owner.

D. Pedestrian Access

1. Sidewalks and Crosswalks
 - a. The main pathway should be minimum of 5'-0" width, of scored concrete. Consideration should be given to adding a grey pigment to the concrete mix.
 - b. Abutting the main pathway there should be a grass or paved strip between the sidewalk and the road, located in DOT ROW where stormwater is managed in an open swale. All street furnishings (trees, plantings, lighting, etc.) should occur within private property. Sidewalks should be located within private property, but they do not have to abut the DOT right-of-way and may be pulled back into the 50' greenway with adequate lighting for pedestrian safety. The Planning Board shall consider requiring site lighting adjacent to the walkways and sidewalks not directly located adjacent to high-visibility right-of-way.
 - c. Clearly marked crosswalks should be placed at all intersections where the sidewalk crosses the site access road.

3.2 Parking

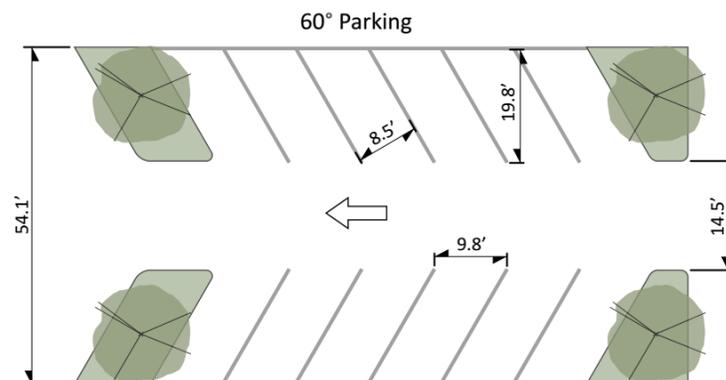
A. Setbacks

The following setbacks shall apply to parking; an access drive to the parking area is allowed within the setback so long as sufficient space for any required vegetation is provided.

1. The parking setback from the front property line along a primary street shall be 50 feet minimum.
2. The parking setback from a property line along a side street shall be 15 feet minimum.
3. The parking setback from a side property line shall be 15 feet minimum and sufficient space for any required vegetation must be provided for.
4. The parking setback from the rear property line shall be equal to the rear yard setback for a building.
5. Where parking is placed between the front building line and Route 67, the parking setback from Route 67 shall be 50 feet minimum and the building setback shall be 100 feet minimum.

B. Parking Spaces

1. Striping shall be in accordance with the MUTCD marking standards.
2. Parallel parking spaces shall be a minimum of 8.5' x 22'.
3. 90° parking spaces shall be a minimum of 9' x 18'.
4. 60° parking spaces shall be a minimum of 8.5' x 19.8'. See graphic below.



C. Shared Parking

1. All required parking spaces may be located offsite, if the parking area is located within 660 feet from the primary entrance of the use served.
2. Up to 50% of the required parking spaces may be located more than 660 feet off-site, if the parking area is located within 1,320 feet from the primary entrance of the use served.

D. Parking Landscaping

See “Landscaping” section for standards.

E. Bike Parking

1. All developments must provide the following minimum bicycle parking spaces:

Size of Use	Required Spaces
0 to 5,000 square feet of gross floor area	2
5,001 to 10,000 square feet of gross floor area	3
10,001 to 30,000 square feet of gross floor area	4
30,001 square feet or more of gross floor area	6

2. Required bicycle parking spaces must be publicly accessible and be located no more than 100 feet from the building entrance to bicycle parking space is intended to serve.
3. Each required bicycle parking space must be a minimum of 2 feet x 6 feet and unobstructed by other site elements or car parking spaces.
4. Racks must be securely anchored, easily usable with both U-locks and cable locks, and support a bicycle at two points of contact to prevent damage to the bicycle wheels and frame.

3.3 Lighting

A. Light Trespass

1. Glare shall be eliminated so that traffic will not be impacted by any lighting installed by the development.
2. Lighting must not be oriented onto adjacent properties, streets or sidewalks.
3. The maximum light level of any light fixture must not exceed 0.1 footcandles measured at the property line at any given point for adjacent commercial property and 0.05 footcandles measured at the property line at any given point for adjacent residential property (including a residential zoning district, a parcel containing a residential use, or residential areas of a PDD).
4. Lights must be “full cut off” or “fully shielded” fixtures.
5. Commercial development should adhere to a “lumen cap” of 25,000 lumens per acre.

B. Design

1. The light source color shall not exceed 3,000 Kelvin.
2. Light fixtures within parking areas may be no higher than 30 feet.
3. Light fixtures within pedestrian areas may be no higher than 15 feet.

Pole-Mounted Lighting Graphics



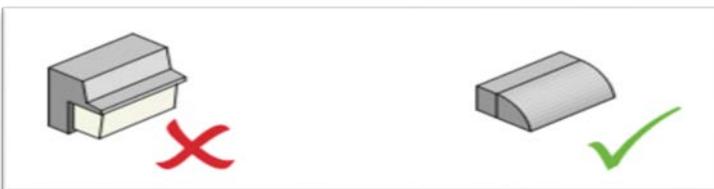
4. Light fixtures located within 50 feet of the property line to a residential zone may be no higher than 15 feet.

Full Cutoff Light Fixture



C. Building and Security Lighting

1. Lighting fixtures must be selected, located, aimed, and shielded so that direct illumination is focused exclusively on the building facade, plantings, and other intended site features and away from adjoining properties and the street right-of-way.
2. All wall pack fixtures must be full cutoff fixtures.



3. Only lighting used to accent architectural features, landscaping or art may be directed upward, provided that the fixture is located, aimed or shielded to minimize light spill into the night sky.

D. Outdoor Sales and Canopy Lighting

1. Outdoor sales and gas station service canopy lighting shall be aimed downward and installed such that the center of the fixture's luminous opening is flush with or recessed into the canopy ceiling. All lighting from the canopy must be substantially confined to the ground area directly beneath the perimeter of the canopy. All exterior lighting for canopies and/or service areas shall be of an indirect nature, emanating only from fixtures located under canopies, under eaves on the principal building or at ground level in the landscaping. Exterior lighting shall be arranged and shielded so there shall be no glare or reflections onto adjacent properties or street rights-of-way.

3.4 Landscaping

A. Parking

1. A perimeter landscape screen must be provided between the street and parking lot. Screen to be a minimum 10-foot wide, landscaped area with a continuous row of evergreen shrubs.
2. Shrubs must be a minimum of 18 inches in height when planted and must reach a minimum size of 36 inches in height within 3 years of planting.
3. Breaks for pedestrian and vehicle access are allowed.
4. A landscaped interior island must be provided every 10 parking spaces. Interior islands must be distributed evenly throughout the parking area. Interior islands may be consolidated or intervals may be expanded in order to preserve existing trees.
5. Each interior island must include at least one shade tree per 150 square feet.
6. In no case can there be less than one shade tree for every 2,000 square feet of parking area including driving aisles.
7. A median island may also serve as the location for a sidewalk. In such case, the sidewalk must be a minimum of 6 feet wide, and the remaining planting area must be no less than 5 feet wide.
8. A coordinated landscape plan shall be provided in order to ensure adequate planting within parking lot interiors and along the perimeter of a lot.
9. Planting islands should be evenly spaced within the parking lot. In order to avoid runoff and allow infiltration, soil or mulch should not be mounded. Innovative and attractive ways to address stormwater management, such as rain gardens, should be encouraged. No planting shall be less than five feet wide in any dimension.

B. Transitional Buffers

1. Applicability

The following transitional buffer requirements shall apply where new development will occur on any parcel located along a property line that abuts a residential zoning district or a property line that abuts a parcel containing a residential use, or that abuts any residential areas of a PDD. These requirements shall not apply to the parcels located near the northwest corner of the intersection of Interstate 87 & Route 67, identified by tax map numbers 229.-2-29.1 and 229.-2-29.2, or the parcels located near the southeast corner of the western side of the State Farm Place and Route 67 intersection, identified by tax map numbers 229.-2-32.2 and 229.-2-32.1.

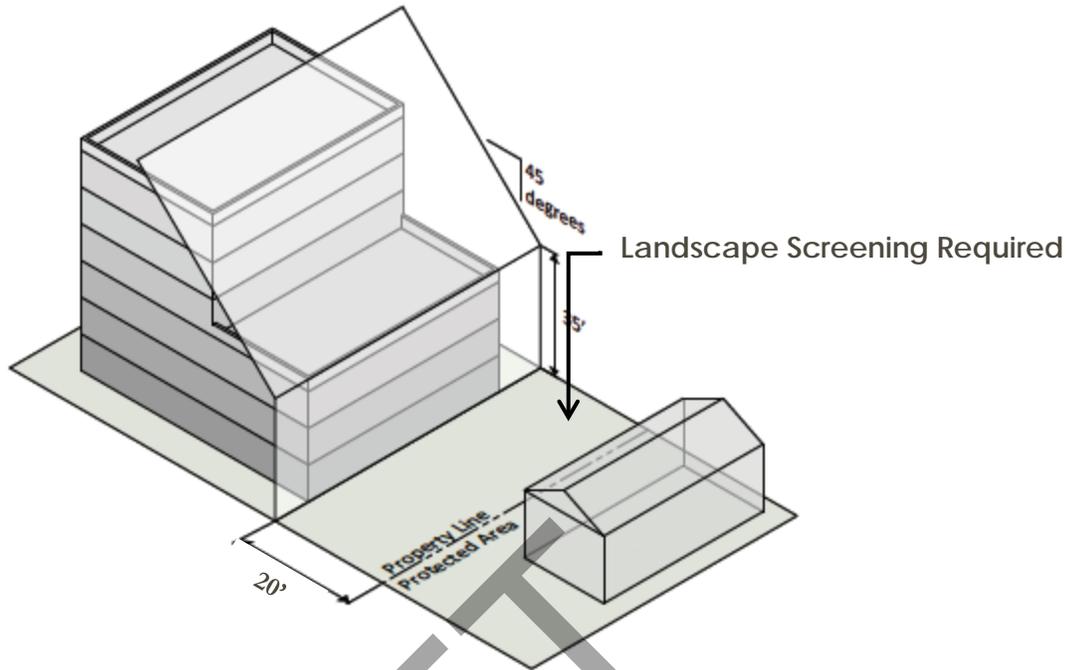
2. General Provisions

- a. A required transitional buffer must be located within the outer perimeter of the lot, parallel to and extending from the property boundary line and must be provided along the entire frontage immediately abutting the property line.
- b. A transitional buffer, as applicable pursuant to section B. 1. shall be provided along the northern boundary of the Overlay District situated north of Route 67, as also depicted on the Transect Map & Table depicted in Section 2.7.
- c. A transitional buffer, unless required pursuant to Section B. 2. B., above, and as otherwise applicable pursuant to Section B. 1., above, shall be provided within the Overlay District, as depicted in Figure A, below.
- d. The width of the buffer is determined exclusive of any required setback; however, the buffer may be located wholly or partially within a required setback.
- e. The parking of vehicles and the placement of buildings is not allowed in a required buffer. All required setbacks apply.

3. Required Buffer

- a. 50% of the required trees must be locally adapted evergreen species.
- b. Shrubs must be evergreen and be of a species that under typical conditions can be expected to reach a height and spread of 4 feet within 3 years of planting. All shrubs must be a minimum of 18 inches tall when planted.
- c. The transitional buffer wall must be constructed of high quality materials including one or a combination of the following: decorative blocks; brick; stone; cat-stone; split-faced masonry blocks; glass block.
- d. A fence may be substituted for the wall. The fence must be constructed of high quality materials including vinyl or wood, with no transparency greater than 20% allowed.
- e. Where native well developed vegetation is available it must be retained on site to be used as the transitional buffer. If none exists, then the transitional buffer described above must be installed. When using existing vegetation, it must be equivalent to the protection afforded by the transitional buffer with larger stature trees, understory and shrub layer. Missing components shall be planted to create an equal buffer. If evergreen shrubs are to be installed per the spacing required, no fence is required. If no shrub layer exists then the fence or wall is required.

The remaining rear setback area shall be left to revegetate naturally to provide a more substantial buffer over time.



Depth (min)	20'
Wall Height (min)	6'
Shade Trees (min per 100')	5
Understory Trees (min per 100')	4
Shrubs (min per 100')	40
Shrub Height (min)	4'

C. Building

1. For every 30 linear feet of building façade, one shade tree must be provided and its trunk should be located within 25 feet of the face of the building. This applies to all four side of a building where the building face is located greater than 25 feet from a transitional buffer or wooded area. The intent is

to soften the visual effect of the architecture, therefore, the trees should be placed along the façade and not grouped into one area, although the Planning Board may have some discretion for placement.

D. Installation and Maintenance

1. General Provisions

- a. Plant materials must be hardy to zone 5, must not be invasive to the area or susceptible to pests known to cause widespread death in the Capital Region (i.e.- Emerald Ash Borer, etc.)
- b. All plants are to be true to species and nursery-grown in accordance with good horticultural practices.
- c. No stands of trees shall be removed unless expressly permitted by the Planning Board in conjunction with a resolution to approve development.

2. Shade Trees – when planted to meet the landscaping Standards

- a. All shade trees must be locally adapted species. Deciduous trees must have an expected mature height of 35 feet or greater and an expected mature crown spread of at least 30 feet or greater and evergreen trees must have an expected mature height of 30 feet or greater and an expected mature crown spread of at least 20 feet or greater unless subject to an overhead line in which case mature height may be less.
- b. All shade trees must, at the time of planting, for deciduous trees, have a minimum caliper of 3 inches and be at least 10 feet tall and for evergreen trees be at least 8 feet tall.

3. Understory Trees – when planted to meet the landscaping Standards

- a. Understory trees must be locally-adapted species with an expected mature height or at least 15 feet and an expected mature crown spread of at least 15 feet.
- b. Single-stem understory trees must, at the time of planting, for deciduous trees, have a minimum caliper of 1.5 inches and be at least 6 feet tall, and for evergreen trees be at least 5 feet tall.
- c. Multi-stem understory trees must, at the time of planting, be at least 6 feet tall for deciduous trees and 5 feet tall for evergreen trees.

4. Maintenance

The owner or tenant is responsible for maintaining all required landscaping in good health or condition. Any dead, unhealthy, deteriorating, broken or missing landscaping must be replaced with landscaping which conforms to this Standard within 30 day (or within 180 days where weather conditions would jeopardize the health of plant materials).

3.5 Signs

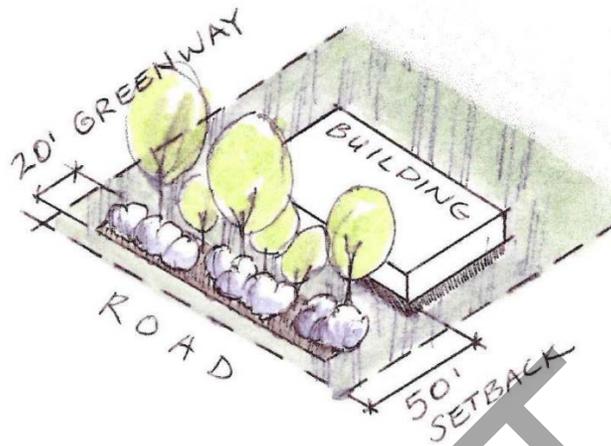
All provisions of the commercial zoning district sign regulations for the C-2 zone apply, the following are additional requirements.

1. Sign materials should be consistent with those used for the adjacent building.
2. Free-standing signs shall be placed so as not to obstruct the view of drivers.

3.6 Greenbelts and Buffers

A greenway should be provided along Route 67 for any new development, with a minimum depth of 20 feet. The greenway can be made up of existing vegetation, planted vegetation, or a combination of the two. The

greenway shall be located between the front property line and the building (within the 50-foot minimum setback), as shown below, and with its terminus at each end meeting or exceeding the extent of pavement or building, whichever extends closest to the side property lines. The minimum composition of the greenway is detailed below:



Greenway Planting Requirement

Depth (minimum): 20 feet

Shade or Evergreen trees (minimum per 30 linear feet): 1

Understory trees (minimum per 30 linear feet): 1

Shrubs (minimum per 30 linear feet): 10

Shrub height minimum at maturity: 30 inches

3.7 Outdoor Amenity Space

A. Requirements

1. Outdoor amenity space must be provided on the lot and must be available as unenclosed, improved active and passive recreational space for use by the occupants, employees or users of the development.
2. Outdoor amenity space must cover a minimum of 5% of the total lot area to be developed (development is defined for this purpose as the area to be covered with structures, pavements, maintained vegetation such as stormwater structures, lawns, required buffers, and pedestrian areas.)

B. Standards

1. Outdoor amenity space may be met in one contiguous open area or in multiple open areas on the lot; however, in order to receive credit the area must be at least 10 feet in width and 10 feet in length.
2. Multiple Lots - A development which aggregates multiple lots into a single development may provide a central amenity space which meets the requirements for the total development so long as it is convenient to the users of the entire site, or may provide multiple spaces so long as they meet the requirements in (1) above.
3. Examples of active outdoor amenities include a playground, athletic court, pool deck, plaza or fountain, promenade or dog park.

4. Passive park areas must include improvements such as trails, paths and seating areas. Formal or informal gardens, greenbelts, ornamental raingardens, and created wetlands designed to appear natural are considered acceptable outdoor amenity areas so long as they are accessible.
5. Outdoor amenity space cannot be parking islands or stormwater features such as ponds, swales and infiltration basins unless they are designed to double as formal or informal garden areas with the use of significant ornamental plantings, seating or other features listed above in (3) or (4).
6. Outdoor amenity space may be roofed but cannot be enclosed. Examples include a pavilion, gazebo or conservatory.
7. Outdoor amenity space may include structures such as kiosks, bicycle racks, benches or other seating, sculpture, fountains and similar amenities. Such areas must not include signs unrelated to the amenities, utility areas, dumpsters or similar structures.
8. Outdoor amenity space cannot be parked or driven upon, except for emergency access and permitted temporary events.
9. Amenities may be seasonal or variable in nature (i.e. outdoor dining, farmer's market, etc.), provided that they occur for a minimum duration of 3 months a year, and are designed to recur permanently.

3.8 Outdoor Elements

A. Outdoor Display

1. Definition

Outdoor display is the outdoor display of products actively available for sale that is placed in a fully-enclosed building at the end of each business day.

Outdoor display does not include merchandise or material in boxes, in crates, on pallets or other kinds of shipping containers, propane gas storage racks, ice storage bins, soft drink or similar vending machines (see outdoor storage).

2. Standards

- a. Outdoor display is permitted in association with any permitted nonresidential principal ground floor use in accordance with the following provisions:
- b. Outdoor display must be removed and placed inside a fully-enclosed building at the end of each business day.
- c. Outdoor display is permitted adjacent to the primary facade with the principal customer entrance, but cannot extend more than 8 feet from the facade and occupy no more than 30% of the horizontal width of the facade.
- d. Outdoor display cannot impair the ability of pedestrians to use the sidewalk or parking areas and must comply with ADA clearance and accessibility.

B. Outdoor Storage

1. Definition

- a. Outdoor storage is the overnight storage of products or materials outside of a building.
- b. Outdoor storage includes merchandise or material in boxes, in crates, on pallets or in shipping containers, propane gas storage racks, ice storage bins, and soft drink or similar vending machines.

- c. Outdoor storage includes the overnight outdoor storage of vehicles awaiting repair, RV's and boats, garden supplies, building supplies, plants, fleet vehicles and other similar merchandise, material, vehicles, or equipment.
2. Standards
 - a. Outdoor storage is not permitted except with Planning Board approval.
 - b. The Planning Board will determine where outdoor storage may be allowed on the site, and its extent (vertically and horizontally) must be shown on the project plan.

3.9 Design Criteria for Special Uses

A. Auto Sales

1. Vehicle display may take place between Route 67 and the building in a limited fashion as determined by the Planning Board.
2. The Planning Board shall establish a location and maximum number of vehicles to be displayed outdoors between the building and Route 67 at a given time.
3. The Planning Board shall establish a location for outdoor sales and storage, if it deems appropriate.
4. Display lighting shall be down lit and shall not exceed an average of 2 fc, see "Section 3.3 Lighting" for further requirements.
5. The design of the display area should be integrated within the site landscaping and screening and a detailed design shall be provided to the Planning Board.

B. Drive-through establishments

1. The Planning Board should ensure that there is adequate stacking space for vehicles within the drive-through and that they are sited appropriately relative to the right-of-way and sidewalks.
2. Drive-through areas and associated structures (menu boards, drive-up window, etc.) shall be located to the side and/or rear of the building.
3. Drive-through lanes must not be placed between the building and the street.

C. Fuel pumps

2. Fuel pumps should be located to the side of the building.
3. Canopies shall utilize the same materials/colors as the building.
4. Maximum illumination, measured in footcandles (fc) should be:
 - a. Gas station approach: 2 fc
 - b. Gas station pump area: average of 5 fc
 - c. Gas station service area: average of 3 fc

D. Loading areas

1. The loading area must be located to the side or rear of buildings. Loading areas may not be placed between the street and the associated building.

ATTACHMENT E

List of Parcels to be Rezoned

PRINT_KEY	PROP_ADDR	OWNER1	MAIL_1ADDR	MAIL_CITY	MAIL_STATE	MAIL_ZIP	ACRES
229.-2-14	10 CANADIAN CT	DeRubertis, Joseph A	10 Canadian Ct	Malta	NY	12020	0.97
229.-2-3.1	705 ROUTE 67	Barnes, Raymond	810 Rock City Rd	Ballston Spa	NY	12020	2.38
229.-2-3.2	697 ROUTE 67	DiDonna, Joseph J	56 Shore Ave	Saratoga Springs	NY	12866	0.07
229.-2-6	682 ROUTE 67	Martin, Ronald F	678 Route 67	Malta	NY	12020	25.27
229.-2-7	678 ROUTE 67	Martin, Ronald F	678 Route 67	Malta	NY	12020	4.40
229.-2-8	672 ROUTE 67	Jenkins, John R	672 Route 67	Malta	NY	12020	5.36
229.-2-9	668 ROUTE 67	Hickey, Thomas	668 Route 67	Malta	NY	12020	6.50
229.-2-10	1 CANADIAN CT	Crandall, Roger W	1 Canadian Ct	Malta	NY	12020	0.94
229.-2-11	2 CANADIAN CT	Guilbault, Lise J	2 Canadian Ct	Malta	NY	12020	0.97
229.-2-12.4	3 CANADIAN CT	Crandall, Roger W	1 Canadian Ct	Malta	NY	12020	0.92
229.-2-27	540 ROUTE 67	Malta Hotel LLC	PO Box 12753	Albany	NY	12212	7.24
229.-2-29.1	600 ROUTE 67	Malta Open Bible Baptist	600 Route 67	Malta	NY	120208106	4.67
229.-2-29.2	604 ROUTE 67	Mountain Shores LLC	604 Route 67	Malta	NY	12020	1.90
229.-2-32.1	633 ROUTE 67	Willette, Shelley M	633 Route 67	Malta	NY	12020	7.41
229.-2-66	4 CANADIAN CT	Guilbault, Andre J	2 Canadian Ct	Malta	NY	12020	2.96
229.-2-75.12	630 ROUTE 67	Church Of The Blessed John, XXIII	40 No Main St	Albany	NY	12203	100.00
229.-2-32.2	639 ROUTE 67	Willette, Ted L	639 Route 67	Malta	NY	12020	2.31
229.-1-33.2	733 ROUTE 67	Duffy, Joseph	733 Route 67	Malta	NY	12020	0.92
229.-1-34	735 ROUTE 67	Carlson, Caren	735 Route 67	Malta	NY	12020	0.48
229.-1-35	432 EAST LINE RD	Vedder, Jeffrey	742 Carlton Rd	Clifton Park	NY	12065	0.57
229.-1-39.3	721 ROUTE 67	Heflin, Garry R	2158 Cook Rd	Charlton	NY	12019	8.31
229.-1-39.11	750 ROUTE 67	Pennacchia Properties Inc	310 Trinacria Ct	Schenectady	NY	12303	11.12
229.-1-39.12	720 ROUTE 67	Fruitful Vine Christian Church	PO Box 1379	Clifton Park	NY	12065	6.22
229.-2-12.5	7 CANADIAN CT	Kozlowski, Robert	204 Brownell Ct	Malta	NY	12020	1.01
229.-2-12.6	8 CANADIAN CT	DeRubertis, Joseph A	10 Canadian Ct	Malta	NY	12020	1.06
229.-2-12.7	CANADIAN CT	DeRubertis, Joseph M	10 Canadian Ct	Malta	NY	12020	1.16
229.-1-66	502 EAST LINE RD	Pennacchia Properties Inc	310 Trinacria Ct	Schenectady	NY	12303	0.57
229.-1-32	726 ROUTE 67	Bartholdi Family	726 Route 67	Malta	NY	12020	1.20
229.-1-33.1	731 ROUTE 67	Kneeland, Donald L	731 Route 67	Malta	NY	12020	0.93
229.-2-13	5 CANADIAN CT	Van Buren, Kevin	5 Canadian Ct	Malta	NY	12020	0.96
229.-2-60.11	ROUTE 67	Martin, Ronald F	678 Route 67	Malta	NY	12020	1.55

210.33

Full Environmental Assessment Form
Part 2 - Identification of Potential Project Impacts

Project :

Date :

Part 2 is to be completed by the lead agency. Part 2 is designed to help the lead agency inventory all potential resources that could be affected by a proposed project or action. We recognize that the lead agency’s reviewer(s) will not necessarily be environmental professionals. So, the questions are designed to walk a reviewer through the assessment process by providing a series of questions that can be answered using the information found in Part 1. To further assist the lead agency in completing Part 2, the form identifies the most relevant questions in Part 1 that will provide the information needed to answer the Part 2 question. When Part 2 is completed, the lead agency will have identified the relevant environmental areas that may be impacted by the proposed activity.

If the lead agency is a state agency **and** the action is in any Coastal Area, complete the Coastal Assessment Form before proceeding with this assessment.

Tips for completing Part 2:

- Review all of the information provided in Part 1.
- Review any application, maps, supporting materials and the Full EAF Workbook.
- Answer each of the 18 questions in Part 2.
- If you answer “**Yes**” to a numbered question, please complete all the questions that follow in that section.
- If you answer “**No**” to a numbered question, move on to the next numbered question.
- Check appropriate column to indicate the anticipated size of the impact.
- Proposed projects that would exceed a numeric threshold contained in a question should result in the reviewing agency checking the box “Moderate to large impact may occur.”
- The reviewer is not expected to be an expert in environmental analysis.
- If you are not sure or undecided about the size of an impact, it may help to review the sub-questions for the general question and consult the workbook.
- When answering a question consider all components of the proposed activity, that is, the “whole action”.
- Consider the possibility for long-term and cumulative impacts as well as direct impacts.
- Answer the question in a reasonable manner considering the scale and context of the project.

1. Impact on Land			
Proposed action may involve construction on, or physical alteration of, the land surface of the proposed site. (See Part 1. D.1)		<input type="checkbox"/> NO	<input type="checkbox"/> YES
<i>If “Yes”, answer questions a - j. If “No”, move on to Section 2.</i>			
Project consists of amendments to the Town Comprehensive Master Plan and Zoning Regulations. No physical changes are proposed.	Relevant Part I Question(s)	No, or small impact may occur	Moderate to large impact may occur
a. The proposed action may involve construction on land where depth to water table is less than 3 feet.	E2d	<input type="checkbox"/>	<input type="checkbox"/>
b. The proposed action may involve construction on slopes of 15% or greater.	E2f	<input type="checkbox"/>	<input type="checkbox"/>
c. The proposed action may involve construction on land where bedrock is exposed, or generally within 5 feet of existing ground surface.	E2a	<input type="checkbox"/>	<input type="checkbox"/>
d. The proposed action may involve the excavation and removal of more than 1,000 tons of natural material.	D2a	<input type="checkbox"/>	<input type="checkbox"/>
e. The proposed action may involve construction that continues for more than one year or in multiple phases.	D1e	<input type="checkbox"/>	<input type="checkbox"/>
f. The proposed action may result in increased erosion, whether from physical disturbance or vegetation removal (including from treatment by herbicides).	D2e, D2q	<input type="checkbox"/>	<input type="checkbox"/>
g. The proposed action is, or may be, located within a Coastal Erosion hazard area.	B1i	<input type="checkbox"/>	<input type="checkbox"/>
h. Other impacts: _____ _____		<input type="checkbox"/>	<input type="checkbox"/>

2. Impact on Geological Features The proposed action may result in the modification or destruction of, or inhibit access to, any unique or unusual land forms on the site (e.g., cliffs, dunes, minerals, fossils, caves). (See Part 1. E.2.g) <input type="checkbox"/> NO <input type="checkbox"/> YES <i>If "Yes", answer questions a - c. If "No", move on to Section 3.</i>			
	Relevant Part I Question(s)	No, or small impact may occur	Moderate to large impact may occur
a. Identify the specific land form(s) attached: _____ _____	E2g	<input type="checkbox"/>	<input type="checkbox"/>
b. The proposed action may affect or is adjacent to a geological feature listed as a registered National Natural Landmark. Specific feature: _____	E3c	<input type="checkbox"/>	<input type="checkbox"/>
c. Other impacts: _____ _____		<input type="checkbox"/>	<input type="checkbox"/>

3. Impacts on Surface Water The proposed action may affect one or more wetlands or other surface water bodies (e.g., streams, rivers, ponds or lakes). (See Part 1. D.2, E.2.h) <input type="checkbox"/> NO <input type="checkbox"/> YES <i>If "Yes", answer questions a - l. If "No", move on to Section 4.</i>			
	Relevant Part I Question(s)	No, or small impact may occur	Moderate to large impact may occur
a. The proposed action may create a new water body.	D2b, D1h	<input type="checkbox"/>	<input type="checkbox"/>
b. The proposed action may result in an increase or decrease of over 10% or more than a 10 acre increase or decrease in the surface area of any body of water.	D2b	<input type="checkbox"/>	<input type="checkbox"/>
c. The proposed action may involve dredging more than 100 cubic yards of material from a wetland or water body.	D2a	<input type="checkbox"/>	<input type="checkbox"/>
d. The proposed action may involve construction within or adjoining a freshwater or tidal wetland, or in the bed or banks of any other water body.	E2h	<input type="checkbox"/>	<input type="checkbox"/>
e. The proposed action may create turbidity in a waterbody, either from upland erosion, runoff or by disturbing bottom sediments.	D2a, D2h	<input type="checkbox"/>	<input type="checkbox"/>
f. The proposed action may include construction of one or more intake(s) for withdrawal of water from surface water.	D2c	<input type="checkbox"/>	<input type="checkbox"/>
g. The proposed action may include construction of one or more outfall(s) for discharge of wastewater to surface water(s).	D2d	<input type="checkbox"/>	<input type="checkbox"/>
h. The proposed action may cause soil erosion, or otherwise create a source of stormwater discharge that may lead to siltation or other degradation of receiving water bodies.	D2e	<input type="checkbox"/>	<input type="checkbox"/>
i. The proposed action may affect the water quality of any water bodies within or downstream of the site of the proposed action.	E2h	<input type="checkbox"/>	<input type="checkbox"/>
j. The proposed action may involve the application of pesticides or herbicides in or around any water body.	D2q, E2h	<input type="checkbox"/>	<input type="checkbox"/>
k. The proposed action may require the construction of new, or expansion of existing, wastewater treatment facilities.	D1a, D2d	<input type="checkbox"/>	<input type="checkbox"/>

I. Other impacts: _____ _____		<input type="checkbox"/>	<input type="checkbox"/>
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4. Impact on groundwater The proposed action may result in new or additional use of ground water, or may have the potential to introduce contaminants to ground water or an aquifer. <input type="checkbox"/> NO <input type="checkbox"/> YES (See Part 1. D.2.a, D.2.c, D.2.d, D.2.p, D.2.q, D.2.t) <i>If "Yes", answer questions a - h. If "No", move on to Section 5.</i>			
	Relevant Part I Question(s)	No, or small impact may occur	Moderate to large impact may occur
a. The proposed action may require new water supply wells, or create additional demand on supplies from existing water supply wells.	D2c	<input type="checkbox"/>	<input type="checkbox"/>
b. Water supply demand from the proposed action may exceed safe and sustainable withdrawal capacity rate of the local supply or aquifer. Cite Source: _____	D2c	<input type="checkbox"/>	<input type="checkbox"/>
c. The proposed action may allow or result in residential uses in areas without water and sewer services.	D1a, D2c	<input type="checkbox"/>	<input type="checkbox"/>
d. The proposed action may include or require wastewater discharged to groundwater.	D2d, E2l	<input type="checkbox"/>	<input type="checkbox"/>
e. The proposed action may result in the construction of water supply wells in locations where groundwater is, or is suspected to be, contaminated.	D2c, E1f, E1g, E1h	<input type="checkbox"/>	<input type="checkbox"/>
f. The proposed action may require the bulk storage of petroleum or chemical products over ground water or an aquifer.	D2p, E2l	<input type="checkbox"/>	<input type="checkbox"/>
g. The proposed action may involve the commercial application of pesticides within 100 feet of potable drinking water or irrigation sources.	E2h, D2q, E2l, D2c	<input type="checkbox"/>	<input type="checkbox"/>
h. Other impacts: _____ _____		<input type="checkbox"/>	<input type="checkbox"/>

5. Impact on Flooding The proposed action may result in development on lands subject to flooding. <input type="checkbox"/> NO <input type="checkbox"/> YES (See Part 1. E.2) <i>If "Yes", answer questions a - g. If "No", move on to Section 6.</i>			
	Relevant Part I Question(s)	No, or small impact may occur	Moderate to large impact may occur
a. The proposed action may result in development in a designated floodway.	E2i	<input type="checkbox"/>	<input type="checkbox"/>
b. The proposed action may result in development within a 100 year floodplain.	E2j	<input type="checkbox"/>	<input type="checkbox"/>
c. The proposed action may result in development within a 500 year floodplain.	E2k	<input type="checkbox"/>	<input type="checkbox"/>
d. The proposed action may result in, or require, modification of existing drainage patterns.	D2b, D2e	<input type="checkbox"/>	<input type="checkbox"/>
e. The proposed action may change flood water flows that contribute to flooding.	D2b, E2i, E2j, E2k	<input type="checkbox"/>	<input type="checkbox"/>
f. If there is a dam located on the site of the proposed action, is the dam in need of repair, or upgrade?	E1e	<input type="checkbox"/>	<input type="checkbox"/>

g. Other impacts: _____ _____		<input type="checkbox"/>	<input type="checkbox"/>
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6. Impacts on Air			
The proposed action may include a state regulated air emission source. (See Part 1. D.2.f., D.2.h, D.2.g) <i>If "Yes", answer questions a - f. If "No", move on to Section 7.</i>		<input type="checkbox"/> NO	<input type="checkbox"/> YES
	Relevant Part I Question(s)	No, or small impact may occur	Moderate to large impact may occur
a. If the proposed action requires federal or state air emission permits, the action may also emit one or more greenhouse gases at or above the following levels: i. More than 1000 tons/year of carbon dioxide (CO ₂) ii. More than 3.5 tons/year of nitrous oxide (N ₂ O) iii. More than 1000 tons/year of carbon equivalent of perfluorocarbons (PFCs) iv. More than .045 tons/year of sulfur hexafluoride (SF ₆) v. More than 1000 tons/year of carbon dioxide equivalent of hydrochloroflourocarbons (HFCs) emissions vi. 43 tons/year or more of methane	D2g D2g D2g D2g D2g D2h	<input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/>	<input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/>
b. The proposed action may generate 10 tons/year or more of any one designated hazardous air pollutant, or 25 tons/year or more of any combination of such hazardous air pollutants.	D2g	<input type="checkbox"/>	<input type="checkbox"/>
c. The proposed action may require a state air registration, or may produce an emissions rate of total contaminants that may exceed 5 lbs. per hour, or may include a heat source capable of producing more than 10 million BTU's per hour.	D2f, D2g	<input type="checkbox"/>	<input type="checkbox"/>
d. The proposed action may reach 50% of any of the thresholds in "a" through "c", above.	D2g	<input type="checkbox"/>	<input type="checkbox"/>
e. The proposed action may result in the combustion or thermal treatment of more than 1 ton of refuse per hour.	D2s	<input type="checkbox"/>	<input type="checkbox"/>
f. Other impacts: _____ _____		<input type="checkbox"/>	<input type="checkbox"/>

7. Impact on Plants and Animals			
The proposed action may result in a loss of flora or fauna. (See Part 1. E.2. m.-q.) <i>If "Yes", answer questions a - j. If "No", move on to Section 8.</i>		<input type="checkbox"/> NO	<input type="checkbox"/> YES
	Relevant Part I Question(s)	No, or small impact may occur	Moderate to large impact may occur
a. The proposed action may cause reduction in population or loss of individuals of any threatened or endangered species, as listed by New York State or the Federal government, that use the site, or are found on, over, or near the site.	E2o	<input type="checkbox"/>	<input type="checkbox"/>
b. The proposed action may result in a reduction or degradation of any habitat used by any rare, threatened or endangered species, as listed by New York State or the federal government.	E2o	<input type="checkbox"/>	<input type="checkbox"/>
c. The proposed action may cause reduction in population, or loss of individuals, of any species of special concern or conservation need, as listed by New York State or the Federal government, that use the site, or are found on, over, or near the site.	E2p	<input type="checkbox"/>	<input type="checkbox"/>
d. The proposed action may result in a reduction or degradation of any habitat used by any species of special concern and conservation need, as listed by New York State or the Federal government.	E2p	<input type="checkbox"/>	<input type="checkbox"/>

e. The proposed action may diminish the capacity of a registered National Natural Landmark to support the biological community it was established to protect.	E3c	<input type="checkbox"/>	<input type="checkbox"/>
f. The proposed action may result in the removal of, or ground disturbance in, any portion of a designated significant natural community. Source: _____	E2n	<input type="checkbox"/>	<input type="checkbox"/>
g. The proposed action may substantially interfere with nesting/breeding, foraging, or over-wintering habitat for the predominant species that occupy or use the project site.	E2m	<input type="checkbox"/>	<input type="checkbox"/>
h. The proposed action requires the conversion of more than 10 acres of forest, grassland or any other regionally or locally important habitat. Habitat type & information source: _____ _____	E1b	<input type="checkbox"/>	<input type="checkbox"/>
i. Proposed action (commercial, industrial or recreational projects, only) involves use of herbicides or pesticides.	D2q	<input type="checkbox"/>	<input type="checkbox"/>
j. Other impacts: _____ _____		<input type="checkbox"/>	<input type="checkbox"/>

8. Impact on Agricultural Resources			
The proposed action may impact agricultural resources. (See Part 1. E.3.a. and b.)		<input type="checkbox"/> NO	<input type="checkbox"/> YES
<i>If "Yes", answer questions a - h. If "No", move on to Section 9.</i>			
	Relevant Part I Question(s)	No, or small impact may occur	Moderate to large impact may occur
a. The proposed action may impact soil classified within soil group 1 through 4 of the NYS Land Classification System.	E2c, E3b	<input type="checkbox"/>	<input type="checkbox"/>
b. The proposed action may sever, cross or otherwise limit access to agricultural land (includes cropland, hayfields, pasture, vineyard, orchard, etc).	E1a, E1b	<input type="checkbox"/>	<input type="checkbox"/>
c. The proposed action may result in the excavation or compaction of the soil profile of active agricultural land.	E3b	<input type="checkbox"/>	<input type="checkbox"/>
d. The proposed action may irreversibly convert agricultural land to non-agricultural uses, either more than 2.5 acres if located in an Agricultural District, or more than 10 acres if not within an Agricultural District.	E1b, E3a	<input type="checkbox"/>	<input type="checkbox"/>
e. The proposed action may disrupt or prevent installation of an agricultural land management system.	E1 a, E1b	<input type="checkbox"/>	<input type="checkbox"/>
f. The proposed action may result, directly or indirectly, in increased development potential or pressure on farmland.	C2c, C3, D2c, D2d	<input type="checkbox"/>	<input type="checkbox"/>
g. The proposed project is not consistent with the adopted municipal Farmland Protection Plan.	C2c	<input type="checkbox"/>	<input type="checkbox"/>
h. Other impacts: _____		<input type="checkbox"/>	<input type="checkbox"/>

9. Impact on Aesthetic Resources The land use of the proposed action are obviously different from, or are in sharp contrast to, current land use patterns between the proposed project and a scenic or aesthetic resource. (Part 1. E.1.a, E.1.b, E.3.h.) <i>If "Yes", answer questions a - g. If "No", go to Section 10.</i>				<input type="checkbox"/> NO	<input type="checkbox"/> YES
	Relevant Part I Question(s)	No, or small impact may occur	Moderate to large impact may occur		
a. Proposed action may be visible from any officially designated federal, state, or local scenic or aesthetic resource.	E3h	<input type="checkbox"/>	<input type="checkbox"/>		
b. The proposed action may result in the obstruction, elimination or significant screening of one or more officially designated scenic views.	E3h, C2b	<input type="checkbox"/>	<input type="checkbox"/>		
c. The proposed action may be visible from publicly accessible vantage points: i. Seasonally (e.g., screened by summer foliage, but visible during other seasons) ii. Year round	E3h	<input type="checkbox"/> <input type="checkbox"/>	<input type="checkbox"/> <input type="checkbox"/>		
d. The situation or activity in which viewers are engaged while viewing the proposed action is: i. Routine travel by residents, including travel to and from work ii. Recreational or tourism based activities	E3h E2q, E1c	<input type="checkbox"/> <input type="checkbox"/>	<input type="checkbox"/> <input type="checkbox"/>		
e. The proposed action may cause a diminishment of the public enjoyment and appreciation of the designated aesthetic resource.	E3h	<input type="checkbox"/>	<input type="checkbox"/>		
f. There are similar projects visible within the following distance of the proposed project: 0-1/2 mile 1/2 -3 mile 3-5 mile 5+ mile	D1a, E1a, D1f, D1g	<input type="checkbox"/>	<input type="checkbox"/>		
g. Other impacts: _____ _____		<input type="checkbox"/>	<input type="checkbox"/>		

10. Impact on Historic and Archeological Resources The proposed action may occur in or adjacent to a historic or archaeological resource. (Part 1. E.3.e, f. and g.) <i>If "Yes", answer questions a - e. If "No", go to Section 11.</i>				<input type="checkbox"/> NO	<input type="checkbox"/> YES
	Relevant Part I Question(s)	No, or small impact may occur	Moderate to large impact may occur		
a. The proposed action may occur wholly or partially within, or substantially contiguous to, any buildings, archaeological site or district which is listed on or has been nominated by the NYS Board of Historic Preservation for inclusion on the State or National Register of Historic Places.	E3e	<input type="checkbox"/>	<input type="checkbox"/>		
b. The proposed action may occur wholly or partially within, or substantially contiguous to, an area designated as sensitive for archaeological sites on the NY State Historic Preservation Office (SHPO) archaeological site inventory.	E3f	<input type="checkbox"/>	<input type="checkbox"/>		
c. The proposed action may occur wholly or partially within, or substantially contiguous to, an archaeological site not included on the NY SHPO inventory. Source: _____	E3g	<input type="checkbox"/>	<input type="checkbox"/>		

d. Other impacts: _____ _____		<input type="checkbox"/>	<input type="checkbox"/>
e. If any of the above (a-d) are answered “Moderate to large impact may occur”, continue with the following questions to help support conclusions in Part 3:			
i. The proposed action may result in the destruction or alteration of all or part of the site or property.	E3e, E3g, E3f	<input type="checkbox"/>	<input type="checkbox"/>
ii. The proposed action may result in the alteration of the property’s setting or integrity.	E3e, E3f, E3g, E1a, E1b	<input type="checkbox"/>	<input type="checkbox"/>
iii. The proposed action may result in the introduction of visual elements which are out of character with the site or property, or may alter its setting.	E3e, E3f, E3g, E3h, C2, C3	<input type="checkbox"/>	<input type="checkbox"/>

11. Impact on Open Space and Recreation			
The proposed action may result in a loss of recreational opportunities or a reduction of an open space resource as designated in any adopted municipal open space plan. (See Part 1. C.2.c, E.1.c., E.2.q.) <i>If “Yes”, answer questions a - e. If “No”, go to Section 12.</i>		<input type="checkbox"/> NO	<input type="checkbox"/> YES
	Relevant Part I Question(s)	No, or small impact may occur	Moderate to large impact may occur
a. The proposed action may result in an impairment of natural functions, or “ecosystem services”, provided by an undeveloped area, including but not limited to stormwater storage, nutrient cycling, wildlife habitat.	D2e, E1b E2h, E2m, E2o, E2n, E2p	<input type="checkbox"/>	<input type="checkbox"/>
b. The proposed action may result in the loss of a current or future recreational resource.	C2a, E1c, C2c, E2q	<input type="checkbox"/>	<input type="checkbox"/>
c. The proposed action may eliminate open space or recreational resource in an area with few such resources.	C2a, C2c E1c, E2q	<input type="checkbox"/>	<input type="checkbox"/>
d. The proposed action may result in loss of an area now used informally by the community as an open space resource.	C2c, E1c	<input type="checkbox"/>	<input type="checkbox"/>
e. Other impacts: _____ _____		<input type="checkbox"/>	<input type="checkbox"/>

12. Impact on Critical Environmental Areas			
The proposed action may be located within or adjacent to a critical environmental area (CEA). (See Part 1. E.3.d) <i>If “Yes”, answer questions a - c. If “No”, go to Section 13.</i>		<input type="checkbox"/> NO	<input type="checkbox"/> YES
	Relevant Part I Question(s)	No, or small impact may occur	Moderate to large impact may occur
a. The proposed action may result in a reduction in the quantity of the resource or characteristic which was the basis for designation of the CEA.	E3d	<input type="checkbox"/>	<input type="checkbox"/>
b. The proposed action may result in a reduction in the quality of the resource or characteristic which was the basis for designation of the CEA.	E3d	<input type="checkbox"/>	<input type="checkbox"/>
c. Other impacts: _____ _____		<input type="checkbox"/>	<input type="checkbox"/>

13. Impact on Transportation The proposed action may result in a change to existing transportation systems. <input type="checkbox"/> NO <input type="checkbox"/> YES (See Part 1. D.2.j) <i>If "Yes", answer questions a - f. If "No", go to Section 14.</i>			
	Relevant Part I Question(s)	No, or small impact may occur	Moderate to large impact may occur
a. Projected traffic increase may exceed capacity of existing road network.	D2j	<input type="checkbox"/>	<input type="checkbox"/>
b. The proposed action may result in the construction of paved parking area for 500 or more vehicles.	D2j	<input type="checkbox"/>	<input type="checkbox"/>
c. The proposed action will degrade existing transit access.	D2j	<input type="checkbox"/>	<input type="checkbox"/>
d. The proposed action will degrade existing pedestrian or bicycle accommodations.	D2j	<input type="checkbox"/>	<input type="checkbox"/>
e. The proposed action may alter the present pattern of movement of people or goods.	D2j	<input type="checkbox"/>	<input type="checkbox"/>
f. Other impacts: _____ _____		<input type="checkbox"/>	<input type="checkbox"/>

14. Impact on Energy The proposed action may cause an increase in the use of any form of energy. <input type="checkbox"/> NO <input type="checkbox"/> YES (See Part 1. D.2.k) <i>If "Yes", answer questions a - e. If "No", go to Section 15.</i>			
	Relevant Part I Question(s)	No, or small impact may occur	Moderate to large impact may occur
a. The proposed action will require a new, or an upgrade to an existing, substation.	D2k	<input type="checkbox"/>	<input type="checkbox"/>
b. The proposed action will require the creation or extension of an energy transmission or supply system to serve more than 50 single or two-family residences or to serve a commercial or industrial use.	D1f, D1q, D2k	<input type="checkbox"/>	<input type="checkbox"/>
c. The proposed action may utilize more than 2,500 MWhrs per year of electricity.	D2k	<input type="checkbox"/>	<input type="checkbox"/>
d. The proposed action may involve heating and/or cooling of more than 100,000 square feet of building area when completed.	D1g	<input type="checkbox"/>	<input type="checkbox"/>
e. Other Impacts: _____ _____			

15. Impact on Noise, Odor, and Light The proposed action may result in an increase in noise, odors, or outdoor lighting. <input type="checkbox"/> NO <input type="checkbox"/> YES (See Part 1. D.2.m., n., and o.) <i>If "Yes", answer questions a - f. If "No", go to Section 16.</i>			
	Relevant Part I Question(s)	No, or small impact may occur	Moderate to large impact may occur
a. The proposed action may produce sound above noise levels established by local regulation.	D2m	<input type="checkbox"/>	<input type="checkbox"/>
b. The proposed action may result in blasting within 1,500 feet of any residence, hospital, school, licensed day care center, or nursing home.	D2m, E1d	<input type="checkbox"/>	<input type="checkbox"/>
c. The proposed action may result in routine odors for more than one hour per day.	D2o	<input type="checkbox"/>	<input type="checkbox"/>

d. The proposed action may result in light shining onto adjoining properties.	D2n	<input type="checkbox"/>	<input type="checkbox"/>
e. The proposed action may result in lighting creating sky-glow brighter than existing area conditions.	D2n, E1a	<input type="checkbox"/>	<input type="checkbox"/>
f. Other impacts: _____ _____		<input type="checkbox"/>	<input type="checkbox"/>

16. Impact on Human Health			
The proposed action may have an impact on human health from exposure to new or existing sources of contaminants. (See Part 1.D.2.q., E.1. d. f. g. and h.) <i>If "Yes", answer questions a - m. If "No", go to Section 17.</i>		<input type="checkbox"/> NO	<input type="checkbox"/> YES
	Relevant Part I Question(s)	No, or small impact may occur	Moderate to large impact may occur
a. The proposed action is located within 1500 feet of a school, hospital, licensed day care center, group home, nursing home or retirement community.	E1d	<input type="checkbox"/>	<input type="checkbox"/>
b. The site of the proposed action is currently undergoing remediation.	E1g, E1h	<input type="checkbox"/>	<input type="checkbox"/>
c. There is a completed emergency spill remediation, or a completed environmental site remediation on, or adjacent to, the site of the proposed action.	E1g, E1h	<input type="checkbox"/>	<input type="checkbox"/>
d. The site of the action is subject to an institutional control limiting the use of the property (e.g., easement or deed restriction).	E1g, E1h	<input type="checkbox"/>	<input type="checkbox"/>
e. The proposed action may affect institutional control measures that were put in place to ensure that the site remains protective of the environment and human health.	E1g, E1h	<input type="checkbox"/>	<input type="checkbox"/>
f. The proposed action has adequate control measures in place to ensure that future generation, treatment and/or disposal of hazardous wastes will be protective of the environment and human health.	D2t	<input type="checkbox"/>	<input type="checkbox"/>
g. The proposed action involves construction or modification of a solid waste management facility.	D2q, E1f	<input type="checkbox"/>	<input type="checkbox"/>
h. The proposed action may result in the unearthing of solid or hazardous waste.	D2q, E1f	<input type="checkbox"/>	<input type="checkbox"/>
i. The proposed action may result in an increase in the rate of disposal, or processing, of solid waste.	D2r, D2s	<input type="checkbox"/>	<input type="checkbox"/>
j. The proposed action may result in excavation or other disturbance within 2000 feet of a site used for the disposal of solid or hazardous waste.	E1f, E1g E1h	<input type="checkbox"/>	<input type="checkbox"/>
k. The proposed action may result in the migration of explosive gases from a landfill site to adjacent off site structures.	E1f, E1g	<input type="checkbox"/>	<input type="checkbox"/>
l. The proposed action may result in the release of contaminated leachate from the project site.	D2s, E1f, D2r	<input type="checkbox"/>	<input type="checkbox"/>
m. Other impacts: _____ _____			

17. Consistency with Community Plans

The proposed action is not consistent with adopted land use plans. NO YES
 (See Part 1. C.1, C.2. and C.3.) [Amendments to Town Comprehensive Master Plan and Zoning Regulations are proposed.](#)
If “Yes”, answer questions a - h. If “No”, go to Section 18. Refer to FEAF Part 3.

	Relevant Part I Question(s)	No, or small impact may occur	Moderate to large impact may occur
a. The proposed action’s land use components may be different from, or in sharp contrast to, current surrounding land use pattern(s).	C2, C3, D1a E1a, E1b	<input type="checkbox"/>	<input type="checkbox"/>
b. The proposed action will cause the permanent population of the city, town or village in which the project is located to grow by more than 5%.	C2	<input type="checkbox"/>	<input type="checkbox"/>
c. The proposed action is inconsistent with local land use plans or zoning regulations.	C2, C2, C3	<input type="checkbox"/>	<input type="checkbox"/>
d. The proposed action is inconsistent with any County plans, or other regional land use plans.	C2, C2	<input type="checkbox"/>	<input type="checkbox"/>
e. The proposed action may cause a change in the density of development that is not supported by existing infrastructure or is distant from existing infrastructure.	C3, D1c, D1d, D1f, D1d, E1b	<input type="checkbox"/>	<input type="checkbox"/>
f. The proposed action is located in an area characterized by low density development that will require new or expanded public infrastructure.	C4, D2c, D2d D2j	<input type="checkbox"/>	<input type="checkbox"/>
g. The proposed action may induce secondary development impacts (e.g., residential or commercial development not included in the proposed action)	C2a	<input type="checkbox"/>	<input type="checkbox"/>
h. Other: _____ _____		<input type="checkbox"/>	<input type="checkbox"/>

18. Consistency with Community Character

The proposed project is inconsistent with the existing community character. NO YES
 (See Part 1. C.2, C.3, D.2, E.3) [Refer to FEAF Part 3.](#)
If “Yes”, answer questions a - g. If “No”, proceed to Part 3.

	Relevant Part I Question(s)	No, or small impact may occur	Moderate to large impact may occur
a. The proposed action may replace or eliminate existing facilities, structures, or areas of historic importance to the community.	E3e, E3f, E3g	<input type="checkbox"/>	<input type="checkbox"/>
b. The proposed action may create a demand for additional community services (e.g. schools, police and fire)	C4	<input type="checkbox"/>	<input type="checkbox"/>
c. The proposed action may displace affordable or low-income housing in an area where there is a shortage of such housing.	C2, C3, D1f D1g, E1a	<input type="checkbox"/>	<input type="checkbox"/>
d. The proposed action may interfere with the use or enjoyment of officially recognized or designated public resources.	C2, E3	<input type="checkbox"/>	<input type="checkbox"/>
e. The proposed action is inconsistent with the predominant architectural scale and character.	C2, C3	<input type="checkbox"/>	<input type="checkbox"/>
f. Proposed action is inconsistent with the character of the existing natural landscape.	C2, C3 E1a, E1b E2g, E2h	<input type="checkbox"/>	<input type="checkbox"/>
g. Other impacts: _____ _____		<input type="checkbox"/>	<input type="checkbox"/>

Project :

Date :

Full Environmental Assessment Form
Part 3 - Evaluation of the Magnitude and Importance of Project Impacts
and
Determination of Significance

Part 3 provides the reasons in support of the determination of significance. The lead agency must complete Part 3 for every question in Part 2 where the impact has been identified as potentially moderate to large or where there is a need to explain why a particular element of the proposed action will not, or may, result in a significant adverse environmental impact.

Based on the analysis in Part 3, the lead agency must decide whether to require an environmental impact statement to further assess the proposed action or whether available information is sufficient for the lead agency to conclude that the proposed action will not have a significant adverse environmental impact. By completing the certification on the next page, the lead agency can complete its determination of significance.

Reasons Supporting This Determination:

To complete this section:

- Identify the impact based on the Part 2 responses and describe its magnitude. Magnitude considers factors such as severity, size or extent of an impact.
- Assess the importance of the impact. Importance relates to the geographic scope, duration, probability of the impact occurring, number of people affected by the impact and any additional environmental consequences if the impact were to occur.
- The assessment should take into consideration any design element or project changes.
- Repeat this process for each Part 2 question where the impact has been identified as potentially moderate to large or where there is a need to explain why a particular element of the proposed action will not, or may, result in a significant adverse environmental impact.
- Provide the reason(s) why the impact may, or will not, result in a significant adverse environmental impact
- For Conditional Negative Declarations identify the specific condition(s) imposed that will modify the proposed action so that no significant adverse environmental impacts will result.
- Attach additional sheets, as needed.

Determination of Significance - Type 1 and Unlisted Actions

SEQR Status: Type 1 Unlisted

Identify portions of EAF completed for this Project: Part 1 Part 2 Part 3

Upon review of the information recorded on this EAF, as noted, plus this additional support information

and considering both the magnitude and importance of each identified potential impact, it is the conclusion of the _____ as lead agency that:

A. This project will result in no significant adverse impacts on the environment, and, therefore, an environmental impact statement need not be prepared. Accordingly, this negative declaration is issued.

B. Although this project could have a significant adverse impact on the environment, that impact will be avoided or substantially mitigated because of the following conditions which will be required by the lead agency:

There will, therefore, be no significant adverse impacts from the project as conditioned, and, therefore, this conditioned negative declaration is issued. A conditioned negative declaration may be used only for UNLISTED actions (see 6 NYCRR 617.d).

C. This Project may result in one or more significant adverse impacts on the environment, and an environmental impact statement must be prepared to further assess the impact(s) and possible mitigation and to explore alternatives to avoid or reduce those impacts. Accordingly, this positive declaration is issued.

Name of Action:

Name of Lead Agency:

Name of Responsible Officer in Lead Agency:

Title of Responsible Officer:

Signature of Responsible Officer in Lead Agency:

Date:

Signature of Preparer (if different from Responsible Officer)

Date:

For Further Information:

Contact Person:

Address:

Telephone Number:

E-mail:

For Type 1 Actions and Conditioned Negative Declarations, a copy of this Notice is sent to:

Chief Executive Officer of the political subdivision in which the action will be principally located (e.g., Town / City / Village of)

Other involved agencies (if any)

Applicant (if any)

Environmental Notice Bulletin: <http://www.dec.ny.gov/enb/enb.html>

TOWN OF MALTA NEIGHBORHOOD 7
Comprehensive Master Plan Amendment, Rezoning and New Route 67 West Overlay

FULL ENVIRONMENTAL ASSESSMENT FORM (FEAF) PART 3
Evaluation of the Magnitude and Importance of Project Impacts.

INTRODUCTION1

1.0 POTENTIAL IMPACT #13 – IMPACT ON TRANSPORTATION1

2.0 POTENTIAL IMPACT #17 – CONSISTENCY WITH COMMUNITY PLANS3

3.0 POTENTIAL IMPACT #18 – CONSISTENCY WITH COMMUNITY CHARACTER5

INTRODUCTION

The Town of Malta is proposing an amendment to the Neighborhood 7 section of the Town of Malta Comprehensive Master Plan adopted in 2005, and a rezoning of parcels along NYS Route 67 consistent with the proposed Comprehensive Plan amendment.

The Town Board appointed a Route 67 West Comprehensive Master Plan Committee on March 21, 2016, to prepare recommendations for zoning and policy changes for the Neighborhood 7 study area (approximately 630 acres). The Committee consists of volunteers, and Committee meetings were conducted at Town Hall with public participation. Collectively, the committee reviewed current zoning, planning documents, policies, plans, reports and surrounding land uses and zoning. Public input was solicited, draft provisions were crafted and revisions to address comment and committee input were completed.

The proposed amendment to the Comprehensive Plan and zoning regulations reflect the Town's desire to plan for and accommodate growth along the Route 67 corridor, while also addressing changing land use characteristics, increases in traffic, and the Town's vision for the corridor, in consideration of the land use plans of adjoining communities.

In order to effect the vision of the Town's Plan, the Town propose to rezoning approximately 210 acres of land to Office Commercial (C-2) and establish the Route 67 West Overlay District to be applied to these rezoned parcels. The Route 67 West Overlay District is crafted specifically for the target area and allows for additional uses, while eliminating other uses which are not consistent with this vision.

1.0 POTENTIAL IMPACT #13 – IMPACT ON TRANSPORTATION

The proposed action may result in a change to existing transportation systems.

- f. *Other Impacts: Proposed action may result in new commercial and residential uses along Route 67 which may generate additional traffic.*

The proposed action consists of an amendment to the Neighborhood 7 Section of the Town of Malta Comprehensive Master Plan adopted in 2005, and associated amendments to Chapter 167, Zoning, of the Town Code that are consistent with the proposed Comprehensive Master Plan changes. The proposed amendment to the Comprehensive Plan and zoning regulations reflect changing conditions of the Town of Malta and the Town's desire to plan for and accommodate growth along the Route 67 corridor, while also addressing traffic issues that have grown over the years. This segment of the Route 67 corridor serves as an important gateway, as this area is bound on the west by the Town's boundary with Ballston at Eastline Road and on the east by the Northway, with the center of Malta just east of the Northway. The proposed action will not result in any new construction or land disturbance, and therefore, will not result in any direct impacts on transportation. Future development of the currently vacant parcels and changes in use of currently developed parcels proposed for rezoning may add additional traffic to Route 67.

The traffic volume along Route 67 is already very high, and while the installation of roundabouts has improved traffic circulation, the improvements have become detrimental to local residents because there are no 'breaks' in traffic flow to allow safe turning movements. Despite NYS DOT's projections of future intersection functionality, traffic is presently problematic and anticipated to become substantially worse as new uses in the Town of Ballston and Village of Ballston Spa develop. The highway surface conditions are poor, and left turns onto or from Route 67 are difficult and hazardous. While it is understood that adding traffic volume to Route 67 may result in further burden to drivers, if no action is taken to address

the under-zoning and subsequent economic impacts to Neighborhood 7 residents, the deterioration of the overall Neighborhood due to traffic impacts will continue, and Malta will not enjoy the benefits of tax base growth, employment opportunities and additional amenities resulting from the introduction of reasonable commercial development along Route 67.

A traffic volume analysis was performed to provide a comparison of potential traffic based on existing zoning to potential traffic based on the proposed zoning amendments. The analysis considered property size and configuration as well as proposed parking and bulk requirements. Based on buildable lots in the proposed area to be rezoned, 11 of the 32 existing lots are considered to have development potential under the existing zoning, resulting in the potential for 63 single family housing units. Due to the required 500-foot front setback from Route 67 with the proposed zoning, only two parcels were evaluated for a mix of commercial and multi-family development, and the remaining parcels were evaluated for commercial use only. Based on this assumption, the potential buildout with the proposed zoning is estimated at 1,236 multifamily housing units, 556,500 SF of commercial space, assuming 3-story buildings with retail on the ground floor (185,500 SF) and office (371,000 SF) on the upper floors. Although higher building heights are permitted for some uses, the parking requirements will drive the overall density.

The following table compares estimated trip generation of potential buildout with existing zoning to potential buildout with proposed zoning for the weekday p.m. peak hour, as described above.

	Existing Zoning	Proposed Zoning	Potential Increase
Enter	45	625	580
Exit	25	800	775
Total:	70	1,425	1,355

The trip estimates for the proposed zoning include an 8% internal trip credit and a 25% pass-by trip credit for the retail use. This volume analysis shows a potential increase of 1,355 vehicle trips during the weekday p.m. peak hour as a result of the proposed zoning amendments at full buildout.

The Saratoga County Regional Traffic Study included an analysis of the Route 67 volumes and capacity thresholds for Existing conditions, Planned Development conditions, and Aggressive Growth conditions. As presented in the study, the Route 67 capacity is 2,000 vehicles per hour (vph) at level of service D (total both directions) and is 2,600 vph at level of service E (total both directions).

The two-way volume in the study for the projected Aggressive Growth condition is 1,950 vph which is at level of service D conditions. Adding the difference in trips between existing zoning and proposed zoning puts the two-way volume on Route 67 at about 3,300 vph which exceeds the 2-lane capacity of Route 67. The estimated capacity at level of service E of Route 67 with a flush median (two-way left-turn lane) is 3,250 vph, slightly less than the projected total volume under the proposed zoning condition.

While the proposed zoning does not directly add traffic to Route 67, subsequent development may add a significant amount of traffic, such that improvement to the mainline capacity will be required to accommodate the projections.

The amendments to the Neighborhood 7 section of the Comprehensive Master Plan include recommendations for improvements to Route 67, as well as the future construction of a new two-way street that runs parallel to Route 67 to provide an alternative for local travelers. While the Comprehensive Master Plan will not directly result in actual improvements, establishing a policy that reflects these needs will provide guidance for future evaluations and/or improvements to Route 67 and/or the overall

neighborhood street network. The Committee also took special consideration of adjoining zoning in the Town of Ballston, as well as recent development approvals in Ballston and Ballston Spa, and deliberated on the impacts of these on the portion of Route 67 in Malta. The proposed zoning amendments will simultaneously take advantage of the presently high and growing traffic volume along the corridor, while taking care to permit uses which would generally be supported by pass-by traffic or uses that would be less likely to generate significant increases to traffic volume. The proposed zoning revisions include provisions related to access management and internal linkage that will help mitigate traffic flows, including shared access and cross access (Section 3.1 of the Route 67 West Overlay Design Standards & Guidelines). The proposed zoning was prepared and recommended by a committee (with public input) that examined the potential impacts to community character, including traffic impacts.

Furthermore, the Town's existing town-wide GEIS examined anticipated significant town-wide traffic volume increases resulting in the adoption of traffic mitigation fees that address potential impacts that are within the GEIS thresholds. These mitigation fees may be used, in part, for improvements in the Rt. 67 corridor. The Town is currently updating the GEIS, which may result in additional traffic mitigation at a town-wide scale. Future development projects will require site-specific SEQR reviews, and any traffic impacts that exceed the thresholds noted in the GEIS will be addressed through specific mitigation measures.

2.0 POTENTIAL IMPACT #17 – CONSISTENCY WITH COMMUNITY PLANS

The proposed action is not consistent with adopted land use plans.

- c. The proposed action is inconsistent with local land use plans or zoning regulations.*
- g. The proposed action may induce secondary development impacts (e.g., residential or commercial development not included in the proposed action).*

The proposed action will not result in any new construction or land disturbance. The proposed action consists of an amendment to the Neighborhood 7 Section of the Comprehensive Master Plan adopted in 2005 and associated amendments to Chapter 167, Zoning, of the Town Code that are consistent with the proposed Comprehensive Master Plan changes. The proposed action includes a rezoning of a portion of the area along Route 67 from residential (R-1 and R-5) to Office Commercial (C-2) with a Rt 67 West Overlay. The Route 67 West Overlay is crafted specifically for the target area and allows for additional uses while eliminating other uses which are not consistent with the visions for this area. The proposed amendment to the Comprehensive Plan and zoning regulations reflect changing conditions of the Town of Malta and the Town's desire to plan for and accommodate growth along the Route 67 corridor, while also addressing traffic issues that have grown over the years. A Route 67 West Comprehensive Master Plan Committee appointed by the Town Board on March 21, 2016, prepared recommendations for zoning and policy changes for the Neighborhood 7 study area. Committee meetings were conducted with public participation. Collectively, the Committee reviewed current zoning, planning documents, policies, plans, reports and surrounding land uses and zoning. Comments and recommendations from the public, which included comments from various professionals such as residential and commercial real estate professionals, were considered and incorporated. It was noted that residential development, especially single-family or two-family, is not marketable in this area along Route 67, and that office and commercial uses may be more viable in this area.

Based on this research, amendments to the Neighborhood 7 text within the Comprehensive Master Plan are proposed. The revised vision and plan for Neighborhood 7 will establish policy to appropriately allow the Town Board to adopt zoning amendments and other related actions in the future. The Committee

identified several elements within the Neighborhood & Comprehensive Master Plan policy which require revision, broader delineation, or new inclusion. The elements that were of highest concern include:

- The need to amend zoning to more appropriately reflect various characteristics of the Route 67 corridor, so that the highest and best use of property may be established in the future;
- Identification of traffic problems that have developed to become of substantially higher concern from what was identified in the 2005 Neighborhood 7 plan;
- Recognition that the segment of the Route 67 corridor serves as an important gateway given its location between the Northway and Town's western boundary at Eastline Road, and the need to ensure high aesthetic quality evolves within this corridor.

The Committee also took special consideration of adjoining zoning in the Town of Ballston, as well as the impacts of recent development approvals in Ballston and Ballston Spa on Route 67 in Malta. The proposed zoning amendments will simultaneously take advantage of the presently high and growing traffic volume along the corridor, while taking care to permit uses which would generally be supported by pass-by traffic or uses that would be less likely to generate significant increases in traffic volume.

Based on an assessment of the recent amendments to C-2 zoning, it was determined that, with appropriate modifications, the C-2 zoning would effectively regulate highway commercial development within Neighborhood 7 along Route 67. The proposed Route 67 West Overlay provisions are similar to the provisions of the C-2 overlay along Route 9, which address design standards, architecture, green space and buffers. Additional provisions of the Route 67 West Overlay regulations address the following issues, as well as other issues:

- Eliminate one-family and two-family development as permitted uses.
- Establish a minimum 50 foot building setback from the Route 67 right-of-way.
- Permit parking in front of buildings, provided that such parking is set back a minimum of 50 foot from the Route 67 right-of-way and provided that a minimum 100 foot building setback is established.
- Include buffer provisions to protect residential uses, and preserve existing trees in setback areas where new development is adjacent to existing residential property, unless otherwise permitted by the Planning Board upon development review.
- Increase maximum lot coverage from 50% to 60% to reduce average development costs in order to promote more cost effective improvements to utility infrastructure.
- Permit multifamily development in the Route 67 West Overlay, with the dimensional requirement that this use is set back a minimum of 500 feet from the Route 67 right-of-way.

The proposed action will encourage reasonable commercial development along Route 67, which addresses the under-zoning and subsequent economic impacts to Neighborhood 7 residents, and provides the benefits of tax base growth, employment opportunities, and additional amenities, consistent with the Town's vision.

3.0 POTENTIAL IMPACT #18 – CONSISTENCY WITH COMMUNITY CHARACTER

The proposed project is inconsistent with the existing community character. (See Part 1. C.2, C.3, D.2, E.3)

e. The proposed action is inconsistent with the predominant architectural scale and character.

The proposed Comprehensive Plan amendment and zoning amendments were prepared by a volunteer committee with public input, and included the evaluation of potential impacts to community character. The proposed action will not result in any new construction or land disturbance, and therefore, will not result in any direct impacts on community character. The area of the proposed action along Route 67 is currently zoned residential (R-1 and R-5) and is partially developed as residential. The proposed action includes a rezoning from Residential to Office Commercial (C-2) and Rt 67 West Overlay. The Route 67 West Overlay is crafted specifically for the target area and allows for additional uses while eliminating other uses which are not consistent with the visions for this area. Therefore, future development of portions of the area proposed for rezoning may be considered inconsistent with existing community character where single family and two-family residences exist. The proposed amendment to the Comprehensive Plan and zoning regulations reflect changing conditions of the Town of Malta and the Town's desire to plan for and accommodate growth along the Route 67 corridor, while also addressing traffic issues that have grown over the years. The proposed action will encourage reasonable commercial development along Route 67, which addresses the under-zoning and subsequent economic impacts to Neighborhood 7 residents, and provides the benefits of tax base growth, employment opportunities, and additional amenities, consistent with the Town's vision. The proposed Route 67 West Overlay incorporates design standards that are expected to improve the aesthetics along Route 67, which will improve community character.